

AGENDA

Planning Commission

Thursday, July 24, 2014 — 7:00 pm

City Hall, Council Chambers, 19100 44th Ave. W., Lynnwood WA

A. CALL TO ORDER – ROLL CALL

B. APPROVAL OF MINUTES

1. Minutes of the May 22, 2014 meeting.
2. Minutes of the June 26, 2014 meeting.

C. CITIZEN COMMENTS – (on matters not scheduled for discussion or public hearing on tonight's agenda) Note: Citizens wishing to offer a comment on a non-hearing agenda item, at the discretion of the Chair, may be invited to speak later in the agenda, during the Commission's discussion of the matter. Citizens wishing to comment on the record on matters scheduled for a public hearing will be invited to do so during the hearing.

D. PUBLIC HEARINGS

1. Mixed Use in the PRC and PCD Zones

E. WORK SESSION TOPICS

1. Draft City Center Streetscape Plan
2. Draft Land Use Element of the Comprehensive Plan

F. OTHER BUSINESS

G. COUNCIL LIAISON REPORT

H. DIRECTOR'S REPORT

I. COMMISSIONERS' COMMENTS

J. ADJOURNMENT

The public is invited to attend and participate in this public meeting. Parking and meeting rooms are accessible to persons with disabilities. Upon reasonable notice to the City Clerk's office (425) 670-5161, the City will make reasonable effort to accommodate those who need special assistance to attend this meeting.

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**CITY OF LYNNWOOD
PLANNING COMMISSION MINUTES
May 22, 2014 Meeting**

Commissioners Present:	Staff Present:
Robert Larsen, First Vice Chair	Corbitt Loch, Dep. Director Comm. Dev.
Chad Braithwaite, Second Vice Chair	Shay Davidson, Administrative Asst.
Maria Ambalada	Gloria Rivera, Senior Planner
George Hurst	Janiene Lambert, City Center Prog. Mgr.
Michael Wojack	
Commissioners Absent:	
Richard Wright, Chair	Other:
Doug Jones	Councilmember Van AuBuchon

Call to Order

The meeting was called to order by Vice Chair Larsen at 7:00 p.m.

Approval of Minutes

1. Approval of minutes of the April 24, 2014 Meeting

Motion made by Commissioner Ambalada, seconded by Commissioner Braithwaite, to approve the minutes of the April 24, 2014 Meeting. Motion passed unanimously (5-0).

Citizen Comments

None

Public Hearing

There was unanimous consent by the Commission to rearrange items A and B to allow the public the opportunity to leave if they wanted.

1. 2014 Comprehensive Plan Potential Amendment List

Vice Chair Larsen reviewed the purpose of the hearing and opened the hearing.

B. 16925 & 17009 – 52nd Ave W (Butler Developments LLC)

Staff Presentation:

Deputy Director of Community Development Corbitt Loch explained that this is a proposed map change to the Future Land Use Map of the Comprehensive Plan Map from Single Family Residential to Multi-family 1 and the Zoning Map from RS-8 to RML along 52nd Avenue NW. Deputy Director Loch explained that staff's recommendation is that this not be recommended for consideration by the Council. The primary reason is that one Comprehensive Plan policy is very clear that no single-family residential property shall be rezoned to any form of multi-family use; except in rare instances, and then only upon showing a clear and convincing evidence of need. On the other hand, the City is working on a larger update of the Comprehensive Plan which is set to come forward in 2015. One of the goals of that larger review is to identify properties that can accept additional population growth because the City of Lynnwood has received an increase in our Population Growth Target from Snohomish County. It would be more appropriate to consider rezoning of this property at that time.

Public Testimony:

Steven Heike, 17011 – 52nd Avenue W, stated that his property abuts 17009 – 52nd Ave W and would be impacted by the rezoning. He expressed concern about increased traffic on their shared driveway.

Jeff Butler, 17009 – 52nd Avenue W, Lynnwood, WA 98037, stated he was present to answer questions and to better explain why he needs to apply for the change in the first place. He acknowledged that Mr. Heike's property is the closest to his and would be the most affected property. The driveway that they share and the fence would not be touched. The reason why he is applying for this is so he doesn't have to worry about the fence. The amount of land he has is sufficient for a multi-family zone. Regarding Deputy Director Loch's comments, he stated that he does have a clear and convincing evidence of need. This property is challenging, and the Code indicates that it can be done if there is evidence of need.

To demonstrate his need he referred to the policy that says, "Land use regulations shall encourage infill housing and redevelopment of underutilized housing sites." He stated that his property is an example of an underutilized housing site. The subject property is five acres, and over half of it is covered with wetlands and the associated buffer. Additionally, completing the required wetlands enhancement is going to cost a significant amount of money. He has explored numerous redevelopment options for his property. All of the information he has gathered over the past several years points to one choice which everyone he has talked to agrees they don't want to see happen there. He said his only option to develop the property, as it sits today without any votes or variances, is

to make a mobile home park with a maximum density of 30 units. This is permitted by the Comprehensive Plan today, but he doesn't want to see this happen.

He builds townhomes and has built three other beautiful developments in Lynnwood already. He explained he would like to build similar-style townhomes to what he has built in the past. He commented that right now he is allowed to build a 30-unit mobile home park which would create traffic. He has five acres with two residential structures. One of the houses is uninhabitable and burned out. One reason the property has never been developed is because it would trigger the expensive wetlands mitigation. He urged the neighbors concerned about his proposal to take a look at the projects he has already done in the city and compare that to the alternative of a mobile home park.

Jean Kramer, 17007 – 52nd Ave W, Lynnwood, WA 98037, said she has lived in her home for almost 27 years. Her neighbor, Steve Heike, has lived east of her for almost 20 years. She explained she does not want any more people using the driveway leading to 52nd Avenue West, which is a very busy road that already has too much traffic on it. Many homeowners with small children would be adversely affected.

Randy Ross, 17007 – 52nd Ave W, stated that the Butler report on the back table says it would be a minimal effect on the surrounding properties and the road. 52nd Avenue in the evening will back up two blocks heading north trying to get through the light at 168th. He expressed concern about additional traffic going up and down the driveway. He stated that any redevelopment of the property, whether it be a mobile home park or multi-family, would adversely affect those who live in that area and those who use 52nd Avenue.

George and Leann Zinka, 17005 – 52nd Avenue W, also expressed safety concerns about the fence and the increased traffic.

Jinman Kwak, 17023 – 52nd Avenue W, expressed concern about safety with the increased traffic and his children playing near the road. He also spoke against both a mobile home park and a multi-family unit. He pointed out that there is also a multi-family development going on south of his house. The construction from that is already creating a lot of noise. He doesn't want to have more.

The public hearing was closed.

Commissioner Wojack asked Deputy Director Loch if an EIS would be required for this project if it does go forward. Deputy Director Loch was not sure if an EIS would be required, but noted that there would be environmental review as required by the state Environmental Policy Act. Commissioner Wojack asked who owns the easement. Deputy Director Loch replied that it is a flag-shaped lot that has a stem going out to 52nd. Commissioner Wojack asked if mitigation can

be used to combine the two sets of driveways to make that one access road. Deputy Director Loch said the issue depends on property ownership.

Commissioner Hurst asked if it was accurate that a mobile home park could be put there. Deputy Director Loch said it was conceivable, but he wasn't sure about the details.

Commissioner Braithwaite asked if the city uses lot-size averaging like the county does. Deputy Director Loch replied that they do.

Commissioner Ambalada commented that a five-acre piece of property is rare in Lynnwood. She believes that perhaps they should consider this an opportunity to create upscale housing.

Vice Chair Larsen commented that his duty is to understand the intent of policies and regulations formed by City Council. Normally, the policy language is not as clear as it is here. He referred to the problematic access issue. He also discussed the upcoming analysis of housing need. It may be something that could happen in the future, but he thinks that with the policy as it stands this would be problematic.

Commissioner Ambalada commented that the policy referred to by Vice Chair Larsen is to deter more people from doing this, but there is a convincing point of need that the owner has mentioned.

Motion made by Commissioner Braithwaite, seconded by Commissioner Ambalada, to remove the Butler Comprehensive Plan Amendment from proposed amendment list being forwarded to Council. Motion passed unanimously.

A. Highway 99 Consistency (City of Lynnwood)

Vice Chair Larsen reviewed the purpose of the hearing and opened the public hearing.

Staff Presentation:

Senior Planner Gloria Rivera explained that the first text amendment is related to two discrepancies in the policies along Highway 99 regarding building heights. Staff's recommendation is to amend the Comprehensive Plan to have the maximum height be six stories. The rationale for the amendment is that this would be consistent with five stories of wood over concrete. At the same time the five stories allows a higher-than-typical housing density in the city's multi-family zones and allows for increased density on Highway 99. The second text amendment would remove the five-acre minimum in the Comprehensive Plan. Staff feels that both amendments fit all criteria for a code amendment.

Commissioner Braithwaite said he was a little disappointed about getting rid of the minimum lot size requirement due to differences in the character of the developments. He would have liked to have seen an incentive for large lots. Also, larger developments might get rid of some of the curb cuts that cause accidents on Highway 99. Senior Planner Rivera commented that it is possible that the existing incentives that accompany increased lot sizes will encourage people to combine lots.

Commissioner Braithwaite commented that originally the transportation nodes were designed to offset the traffic impacts of multi-family development. Now that multi-family is effectively allowed all along Highway 99, the traffic impacts will not be mitigated as planned. Deputy Director Loch replied that the City Council has directed the changes that they saw fit including the multi-family along the corridor and not just at the nodes. Additionally, if there is a multi-family development at a spot not on a node along Highway 99, it doesn't necessarily mean it would generate more trips than multi-family at a major intersection. This is something that could be studied by traffic engineers.

Vice Chair Larsen commented that tonight is really just about whether or not this should be recommended to be put on the list for consideration. The public hearing was closed.

Motion made by Commissioner Braithwaite, seconded by Commissioner Ambalada, to recommend sending this item (the Highway 99 Subarea Plan) to the City Council with a recommendation to be included on the proposed amendment list. Motion passed unanimously.

Work Session

1. City Center Project Prioritization

City Center Program Manager Janiene Lambert explained that on March 31, 2014 the City Council informally approved the evaluation criteria and made their comments. Following that, staff took the next steps to prioritize the projects by scoring each based on the evaluation criteria and identifying funding partnerships and linkages. She then presented the City Center Project Prioritization Package as contained in the Planning Commission's packet.

Commissioner Braithwaite asked what category a catalyst project would fit into. Ms. Lambert replied that the project linkages page is a good example of that. This shows how the City projects could be partnered with private development to catalyze greater private investment.

Commissioner Ambalada asked what needs to happen to make the Promenade a reality. She thinks the Promenade would be a reason for people to come to the

area. Ms. Lambert explained there are a couple components of the Promenade in the Sub-Area Plan. The Promenade Street is a publicly-owned right-of-way at 198th Street SW from 44th Avenue West to 40th Avenue West. The pedestrian portion of the Promenade is incentivized through FAR bonus with redevelopment. Staff is supportive of that vision as redevelopment occurs.

Commissioner Braithwaite asked what the 196th Pedestrian Crossing was. Ms. Lambert explained it was envisioned to be a pedestrian crossing over 196th to link with the convention center. This was originally envisioned with a former hotel proposal. It's on the list as a possibility because a hotel proposal could still happen in the future. It also provides additional access for the Promenade.

Commissioner Wojack said he would like to see a medium-sized theater as the City Center grows. Ms. Lambert referred to question 2 which explains how the City would integrate potential new projects in future updates. She noted that a performing arts center has also preliminarily been discussed with the Arts Commission.

Vice Chair Larsen said he hopes people who are following this project understand the significance of the prioritization list. He commended staff for this accomplishment. He commented that one thing that still needs to be achieved in the City Center is a sense of place. Ms. Lambert explained that the light rail helps create a place by increasing accessibility. This is why transit-oriented development is on the list. The project linkages map is also critical. Creating a main street also helps to create a sense of place. Additionally, there would be a park in the main street area.

Other Business

None

Council Liaison Report

Councilmember AuBuchon stated that a public hearing on miniature goats will be held at Council next week. There will be three other public hearings on the same evening, including two involving marijuana. He thanked the Commission for their hard work on the miniature goat issue.

Director's Report

Deputy Director Loch informed that Commission that Director Krauss was at a Community Safety and Health meeting tonight and was unable to come.

Commissioners' Comments

Commissioner Wojack thanked Deputy Director Loch for the permit fees comparison.

Adjournment

The meeting was adjourned at 8:45 p.m.

Richard Wright, Chair

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**CITY OF LYNNWOOD
PLANNING COMMISSION MINUTES
June 26, 2014 Meeting**

Commissioners Present:	Staff Present:
Michael Wojack, Acting Chair	Corbitt Loch, Dep. Director Comm. Dev.
Maria Ambalada	Gloria Rivera, Senior Planner
George Hurst	Shay Davidson, Administrative Asst.
Doug Jones	Robert Victor, Project Manager
	Jeff Elekes, Deputy Director of PW
Commissioners Absent:	
Chad Braithwaite, Second Vice Chair	
Robert Larsen, First Vice Chair	Other:
Richard Wright, Chair	Councilmember Van AuBuchon

Call to Order

The meeting was called to order by Chair Wright at 7:00 p.m.

Approval of Minutes

None

Citizen Comments

Public Hearing

1. Regulations relating to recreational marijuana

The hearing was opened at 7:03 p.m. for regulations relating to recreational marijuana.

Staff Presentation: Senior Planner Gloria Rivera reviewed background on this item as contained in the Staff Report in the Planning Commission's packet. The Ordinance under consideration was modeled after Pierce County's ordinance based on a recommendation from the City Attorney. This specifies activities in set zones, but makes the licensing and permitting of production, processing, and retail sales contingent on the federal government approving legislation that removes marijuana from the schedule of controlled substances. Senior Planner Rivera gave a PowerPoint presentation describing details of the Ordinance as contained in the Staff Report.

Public Comments:

Matthew Moe, 727 – 35th Street, Everett, WA, stated he is one of the fortunate few who has a right to a license here in Lynnwood. He explained he has been working in the healthcare industry for the last decade or more with a focus on product development, business development, and strategic planning. He believes that what is going on with I-502 is the right thing since it offers a solution to the current status. The prohibition that has occurred for the last decades has clearly failed and cost the nation over a trillion dollars in enforcement costs. He thinks a regulated system would provide a safer situation for everyone. He stressed that the City needs to be proactive in accommodating this law like many other cities around us have done. He asserted that a ban would actually increase crime. Right now, citizens have a legal right to access marijuana, but they would have no legal access point within the City. A ban would force citizens to go to the black market to find their product rather than seek legal products in the regulated market. He urged the Commission to carefully consider the ban. He thinks accommodation would be a far better solution with more positive impacts on public health.

Magda Clemens, 104 NE 50th Street, Seattle, WA, spoke in support of legalizing access to recreational marijuana which she believes will increase safety for children and for those seeking access to marijuana. She believes that I-502 has been well considered and will prioritize the health of the public. The Ordinance provides consideration for daycares and public places such as libraries and other areas where children may be present so that children do not have easy access. She stressed the importance of educating children on marijuana and the uses of it versus making it illegal. She noted that the ban would force good, law abiding parents to enter the black market to access marijuana and would increase risks to their safety.

Stephanie Boehl, Attorney for Matt Moe/Green Express, LLC, 12246 – 12th Ave NW, Seattle, stated that Mr. Moe is one of the applicants who were selected in the retail lottery and is hoping to open a retail store in Lynnwood. She discussed revenue projections using data from Rand, estimating that at a minimum a retail store would bring in about \$5.2 million a year. At this rate, two stores would bring sales tax revenue to the City of Lynnwood of \$300,000 a year. She thinks this is a very conservative estimate and would likely be closer to \$500,000. She stated that the proposed ban means Lynnwood would be turning its back on \$500,000 in tax revenue. She asked if the city's fears could be addressed by something other than a ban. The ban would create an unregulated, untaxed, untested market as the only means available to Lynnwood citizens to access marijuana. She pointed out that there are contrary studies to all the ones listed in the Ordinance and the exhibits. Additionally, the citizens of Lynnwood voted 54% in favor of a taxed, legitimate, available source of safe marijuana in their community. For the City to turn its back on that is problematic and against the democratic values of our country and the state's initiative process. She stated

that Matt Moe and her other clients are legitimate business people who want to do something great here. She left copies of the CDC report in the Huffington Post and copies of the Drug Policy Alliance report with the Commission.

Charles Austin, 18930 – 43rd Avenue SE, Lynnwood, WA 98012, stated that it was actually 56% that voted to legalize marijuana in I-502. This action by the City feels like we are taking another step back. He suggested they stop arresting people and start profiting from the industry. He spoke in support of allowing responsible adults to be treated like legal, responsible adults by going to a state store to purchase a product that we know is verifiable and safe. He encouraged the City to focus on receiving the tax revenue from this industry instead of spending money on arrests and prosecutions.

Planning Commission Questions:

Commissioner Hurst asked how the banking regulations and the ability to use the banking system for this industry have shifted. Ms. Boehl replied that the federal government put out an enforcement policy on banks that opened accounts for licensed retail businesses. Two credit unions in Washington State are putting together their proposal for retail stores right now. The Liquor Control Board is also involved in this process as they will be receiving a lot of revenue from this industry. She stated that the business's she represents are opening and using accounts at the credit union. She feels that great strides have been made in the last several months.

Charles Austin said he would like to see this taken out of the black market area and stop supporting the drug cartels. He spoke in support of an organized, regulated system that was voted in.

Seeing no further questions or public comments, the public testimony portion of the hearing was closed at 7:42 p.m.

Planning Commission Deliberation:

Commissioner Jones asked if there is a way of amending the City's business license to remove the line stating that you cannot violate federal law or to add an amendment stating, ". . . unless approved by state law." Senior Planner Rivera commented that it could be amended if it goes through the appropriate process.

Commissioner Hurst asked hypothetical questions about the 1,000-foot minimum distance regulation and the impact of future development on retail stores in which Senior Planner Rivera clarified this for him. Commissioner Hurst asked how the 10,000 square foot limitation for grow operations was determined. Senior Planner Rivera stated the City had followed the lead of another jurisdiction on this, but it relates to one of the tiers in the WAC. Commissioner Hurst asked if the energy usage of a growing operation had been investigated. Senior Planner Rivera said

it had not. Commissioner Hurst commented that they use a tremendous amount of energy, and power companies are struggling to deal with this.

Commissioner Ambalada asked if staff had looked into potential revenue projections that the City would get from marijuana businesses. Senior Planner Rivera stated that it had not been studied thoroughly. Commissioner Ambalada said she was wrestling with the economic value of this and the economic growth of the City of Lynnwood. She discussed the importance of family values to the City of Lynnwood and spoke in support of the Ordinance.

Commissioner Wojack referred to section 1, item f, stating that no outdoor signage would be allowed for a marijuana store. He wondered if sandwich boards would be allowed. Senior Planner Rivera indicated she would need to look into that further. Commissioner Wojack asked if the City is worried about the possibility of being a scapegoat for the federal government. Senior Planner Rivera said she wasn't aware of any worry on the part of the Council, but there are a couple of cities being sued right now because they have prohibited it. She added that Lynnwood would not be the only city in the county that has prohibited it.

Commissioner Jones spoke in support of the Ordinance, but said he'd like to see more of a collaborative effort between the business owners and the City.

Motion made by Commissioner Ambalada, seconded by Commissioner Hurst, to forward the Ordinance to Council with a recommendation to continue prohibition.

Motion made by Commissioner Jones to amend the Ordinance by striking the phrase in the Ordinance prohibiting licensing and the section that references the federal legislation and controlled substances. The motion to amend was seconded by Commissioner Wojack. Motion failed (2-2).

The main motion failed (2-2).

After some discussion, Deputy Director Loch explained that the next step would be to return this item to the City Council indicating that the Planning Commission was not able to reach a recommendation.

Other Business

1. 2015-2020 Transportation Improvement Plan (TIP)

Robert Victor, Project Manager, and Jeff Elekes, Deputy Director of Public Works, reviewed the annual update to the 2015-2020 City's Transportation Improvement Project (TIP) list and map in the Commission's packet. Mr. Victor noted that the only change from last year's TIP, other than updating the dollar figures, is that one project has been removed and one project has been added to

the list. In 2014, the ITS Phase 3 project will be completed. The citywide Bike Link Project was added to the list. Staff recommended forwarding this to the City Council with a recommendation for approval.

Commissioner Wojack referred to the Beech Road Extension and asked about the purpose of that. Deputy Director Elekes explained that the Beech Road Extension is intended to make the linkage extension behind Target and to be an alternate route for the mall. It is intended to come on line at some point in the future as growth occurs in the City Center.

Commissioner Wojack asked about the plans for the 33rd Avenue West Extension. Deputy Director Elekes explained that the "Ring Road" will start at 33rd, go north, wrap around, and go down the hill to the AM/PM market. The other one will make a connection to Maple Road through 179th, but this is a later project.

Commissioner Ambalada asked if there are any plans to widen 52nd. Deputy Director Elekes said there is in the long-range plan. As development occurs on the frontage, it is required to put in its share of frontage improvements to extend sidewalks or create a gap. In the long-range, subject to project funding, there is a non-motorized sidewalk plan.

Commissioner Jones asked several questions about funding and bidding. Deputy Director Elekes and Mr. Victor reviewed the funding for the projects. They explained that the bidding is open to the public if someone wants to review the records.

Commissioner Hurst asked if the City has ever spent \$2 million a year on the overlay program. Deputy Director Elekes replied that through the capital projects such as Olympic View Drive, there was over \$2 million worth of asphalt put down in that year on that project. Other than that, the City has not had the budget to do a \$2 million program. The Transportation Benefit District (TBD) Board will be considering a voter package in July to be able to do an annual overlay program of substance. Commissioner Hurst then asked about recommendations for roundabouts. Mr. Victor explained that roundabouts are only advisable in certain circumstances. It depends on the type of traffic coming into the intersection. There are only a few select locations in the City where roundabouts make sense.

Commissioner Ambalada asked about ADA access on the Interurban Trail. Mr. Victor affirmed that the project will meet ADA requirements in order to receive the federal funding associated with it.

Commissioner Hurst asked if bids are still coming in under budget like they were a few years ago. Mr. Victor explained that the City is not seeing as many bidders as they were a few years ago. It is a more normal bidding pattern. The engineers' estimates and the bids are on par with one another.

Commissioner Ambalada asked if there is something that the Planning Commission can help staff with. Mr. Victor explained that funding is the thing that the City needs most. Support for the ballot measure would be appreciated. Also helpful would be talking with the legislature about getting Lynnwood its fair share of state-funded packages and programs that are out there.

Motion made by Commissioner Wojack, seconded by Commissioner Jones, to forward the 2015-2020 TIP as written. Motion passed unanimously (4-0).

Work Session

1. Preliminary Draft – Land Use Element of Comprehensive Plan

Deputy Director Loch explained staff is looking for feedback on the format and the style of Land Use Element.

Commissioner Wojack said he thought it was excellent. He appreciated the explanation of the proposed changes. He thought it was very smooth and made sense.

Commissioner Ambalada said she wished there was more blue ink than red.

There was consensus to continue this to a future meeting when more commissioners were present.

Council Liaison Report

Councilmember AuBuchon had the following comments:

- The revision of the Land Use Element shows that the entire Lynnwood Municipal Code could probably use this kind of work. He thanked staff for that work.
- He announced that all of the Commission's work on the goat issue has come to fruition. At the last Council meeting, they voted to allow miniature goats with some minor changes to the size and breeds of the goat. He thanked the Planning Commission for their work.

Commissioner Jones asked how the planning for joint meetings is coming along. Councilmember AuBuchon replied he did not have an answer for that yet, but said he would pass along the question. Commissioner Jones asked staff if they had any thoughts on this because he thought that it would save staff time. Deputy Director Loch suggested not worrying about staff on this, noting that they were happy to help facilitate city business. He noted that it was more about whether or not the facilitation of this could benefit by joint meetings. He suggested that the Planning Commission could discuss at their next meeting the

types of topics or instances that would benefit from joint meetings. There was consensus to have this as an agenda item at an upcoming meeting.

Director's Report

Deputy Director Loch had the following comments:

- He encouraged everyone to go to the new Farmer's Market every Thursday from 3:00 pm to 7:00 pm.
- Staff will be working on the Business License Code and will be providing informative updates to the Planning Commission even though it wouldn't require Planning Commission action.

Commissioners' Comments

Commissioner Wojack thanked the Commission for dealing with him as Acting Chair at the meeting. He noted that there were two firsts at this meeting, the marijuana product and also the first time in his 10 or 11 years as a commissioner that there has ever been a tie on a vote.

Adjournment

The meeting was adjourned at 8:57 p.m.

Richard Wright, Chair

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**Mixed Use In the PRC & PCD
Zones**

Agenda Item: D.1

Staff Report

Staff Contact: Corbitt Loch, Deputy Director

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- Public Hearing
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- Work Session
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- Other Business
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- Information
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- Miscellaneous

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Summary

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This agenda item provides a public hearing for a draft code amendment prepared in concert with the Planning Commission during 2012 and 2013. The City Council recently authorized the scheduling of public hearings for this matter, and so the draft legislation is returned to the Planning Commission for a hearing and recommendation as normal procedure for amendments to the Zoning Code. The draft ordinance provided herein is essentially the same as last seen by the Planning Commission in 2013.

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The draft amendments relate to mixed-use development upon commercially-zoned property—particularly the Planned Retail Center zone (PRC) and Planned Commercial Development zone (PCD).

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PRC zoning applies to the Alderwood Mall and also to commercial properties to the east and west (i.e., Toys R Us, Kohl's, TJ Max and Albert Lee Appliance). Meanwhile, properties zoned PCD include other prominent commercial properties to the north and south of the Mall (i.e., H Mart, Total Wine, Target, Marshall's, Whole Foods and Embassy Suites.). A map of PRC and PCD zoning is provided as an attachment.

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Following the Planning Commission's hearing and recommendation, another public hearing will be scheduled before the City Council. At the conclusion of that second hearing, the City Council is expected to take final action on the draft legislation.

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While the existing PRC and PDC regulations currently allow multifamily dwellings within a mixed-use project, those regulations contain stipulations make mixed use virtually infeasible. The proposed amendments to development regulations for the PRC and PCD zones can be summarized as follows:

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- Repeals prohibition of multifamily dwellings on the second and third building levels.

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- Repeals the requirement that floor area dedicated to residences not exceed the amount of floor area used for non-residential purposes.

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- Repeals the 35 percent maximum lot coverage provision for the PRC and PCD zone.

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- Adds requirements for onsite recreation area/open space for mixed use in commercial zones regulated by chapter 21.46 LMC. Applicable to the B-1, B-2, B-3, C-G, PCD and C-R zones. Separate regulations apply to the HMU and City Center zones.

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1 **Policy Question(s)**

- 2 • Are the proposed code amendments appropriate to allow the potential for
3 additional residential capacity in the vicinity of the Mall?

4 **Action**

5 Conduct the public hearing and receive public testimony. Upon the close of the public
6 hearing, recommend that the City Council either: a) approve the draft Ordinance as
7 written; or b) approve the draft Ordinance as amended; or c) not approve the draft
8 Ordinance.

9 **Background**

10 This draft legislation was initiated to better align Lynnwood's land use regulations with
11 the practices, requirements and norms of: building construction, development financing,
12 regional policies for urban growth, and the policies of the Lynnwood Comprehensive
13 Plan. Another impetus for the ordinance is evolving societal norms and preferences with
14 regards to urban form, housing, and lifestyle.

15 In recent years, a number of events have collectively altered the context of the City's
16 retail core. It is appropriate to periodically adjust Lynnwood's development regulations
17 so that our regulations help realize the community's vision for the future, not community
18 ideals of years' past. Local events specific to the Mall vicinity include:

- 19 • City Center redevelopment is moving forward with 308 senior housing
20 apartments approved for construction (on 40th Ave W), and an additional 339
21 apartments and a 6-story hotel are proposed (ESD property).
- 22 • The light rail transit station and service line are in design for extension to
23 Lynnwood (completion 2023). Meanwhile, work has started on conceptual
24 design for extension northward to Everett, which is to include transit stops in City
25 Center and at the Mall.
- 26 • Design is underway for the planned Poplar Way Bridge. Funding has been
27 approved for both design and property acquisition. This new bridge will make
28 33rd Ave W a primary entryway to the Mall neighborhood.
- 29 • Construction has begun on the Lynnwood Place project, containing nearly one
30 million sq. ft. of building floor area, including up to 500 apartments. These
31 dwellings will be in mixed use buildings (over street-level retail). Lynnwood
32 Place includes an extension of 33rd from 184th to Alderwood Mall Parkway, which
33 will help to relieve local traffic congestion.
- 34 • Development regulations for the Transition Area are currently under
35 consideration by the City Council. At a minimum, it is expected that new
36 Transition Area regulations will support redevelopment along 33rd Ave W.

37 Collectively, these events demonstrate that there is strong interest in new construction in
38 the area of Alderwood Mall. When economic and social trends are favorable to new
39 investment, it is appropriate to confirm that development regulations will help realize
40 Lynnwood's community vision. The legislation proposed is intended to keep Lynnwood's
41 development regulations current.

1 The topic of mixed use development in the retail core was discussed by the Planning
2 Commission on May 24, 2012, February 14, 2013, February 28, 2013 and March 28,
3 2013 (meeting minutes attached). The draft ordinance was developed and refined
4 based upon direction provided by the Commission. The City Council reviewed the draft
5 legislation on March 31, 2014, and instructed staff to proceed with public hearings. City
6 staff has also completed environmental review (SEPA) and also notified the Department
7 of Commerce as required by State law. The impetus for these code changes and
8 descriptions of those changes are further described below:

9 Population Growth Targets: The City of Lynnwood has adopted policies to preserve and
10 protect existing single family residential areas. The City is also required by the State
11 Growth Management Act (GMA) to accommodate a portion of the region's future
12 population and employment growth. Draft population growth targets call for Lynnwood to
13 accommodate nearly 10,000 additional persons beyond City's current
14 projection/capacity. As a result of the 2006 Growth Targeting initiative, Lynnwood
15 undertook significant steps to accommodate projected growth including the City Center
16 Sub-Area Plan and the Highway 99 Corridor Plan. However, consistent with State law,
17 growth targets are now being extended out to 2035 and all cities and counties in the
18 region are required to accept their allocation as specified by Countywide Planning
19 Policies.

20 As a "built-out" community, there are relatively few areas where increased population
21 can be realized. The City's Comprehensive Plan has long been predicated upon
22 preserving single family neighborhoods. In contrast, Snohomish County took the
23 alternative approach and controversial approach of allowing higher density development
24 (LDMR's) in single family neighborhoods.

25 The Mall vicinity is predominately 1-2 story structures amidst large fields of parking. This
26 area of Lynnwood has considerable public infrastructure (roads, utilities, sidewalks, etc.)
27 and a fundamental premise of the GMA is to site new development where essential
28 infrastructure exists (or is planned). Currently, planning is underway for important
29 infrastructure improvements such as: Poplar Way Bridge, upgrades to sewer Lift
30 Stations 4 and 8, Lynnwood Place's perimeter road, and light rail service. Compared to
31 our existing single family residential neighborhoods and for all of these reasons, the Mall
32 area seems to be an appropriate place for population growth.

33 Regional Growth Center: The vicinity of the Mall and the City Center is designated as
34 the Lynnwood Regional Growth Center by PSRC's VISION 2040. This regional plan for
35 future growth steers the majority of new development to designated urban centers,
36 growth centers, and core communities. Infrastructure improvement projects that serve
37 Regional Growth Centers are likely to score well for competitive funding.

38 Urban Form: Many communities with regional retail centers are taking steps to
39 incorporate new housing in areas that were singularly retail in nature. This approach is
40 intended to accomplish goals such as: a) reduce society's dependence upon the
41 automobile by reducing the distance between homes, shopping, and employment; b)
42 establish land use density/intensity that can support transit service; c) promote healthy
43 lifestyles by facilitating walking and bicycling; and d) reducing infrastructure costs (on a
44 per-capita basis) by minimizing the lineal distance of roads and utilities.

45 This sort of "maturation" of malls and commercial centers is being experienced
46 throughout the country and our own region. One need only look to the Northgate Mall
47 area for an example. The vital components are the conversion of existing large parking
48 fields into structured parking thereby creating new building capacity plus replacing what

1 may be single-story or low-rise structures with multistory building designs. In addition to
2 the factors mentioned above, supporting mixed use development in these areas is a
3 reasonable strategy since:

- 4 • Malls and surroundings have already had major investments in transportation
5 infrastructure. In the case of Alderwood, future projects include the Lynnwood
6 Place loop road that will improve flow through existing traffic bottlenecks, the
7 Poplar Way bridge over I-5 linked to 33rd Ave. the will create a major new link to
8 I-5 greatly improving access into the area and pulling traffic away from single
9 family neighborhoods to the west, major transit service that is already provided
10 including the new CT route from Edmonds, along 196th and terminating at
11 Alderwood, and if the voters approve ST3, a light rail station that will serve the
12 area.
- 13 • In accordance with City Plans the Alderwood commercial area is reasonably
14 separated and buffered from neighborhoods.
- 15 • Mixing residential and commercial financially benefits area businesses while
16 creating a neighborhood that is active 24/7. The residential development
17 supports commercial activity to meet the needs of the residents.
- 18 • Mixed use residential and overall development intensification can be
19 accomplished without detracting from the areas already high level of commercial
20 activity.
- 21 • There already appears to be some developer interest in the concept.

22 The new Lynnwood Place mixed-use development (immediately north of the Mall) will
23 include dwellings built over retail space at street-level, and configured in a compact,
24 walkable, urban setting. When built, this project will significantly alter the character of
25 Mall neighborhood. The draft code amendments would allow comparable types of
26 development on other nearby properties (with appropriate mitigation of adverse
27 impacts).

28 When considering that: a) the vicinity that includes the Mall and City Center is
29 designated as a VISION 2040 Regional Growth Center; and b) today's shoppers are
30 attracted to retail centers offering a more-varied experience, staff expects the continued
31 evolution of the Mall area to include:

- 32 • Stores clustered along pedestrian promenades or sidewalks.
- 33 • Dwellings constructed over street-level shops and services, giving: a) residents
34 access to goods and services without the need for an automobile; and b)
35 businesses a steady, affluent market base.
- 36 • Multi-level parking structures providing ready access to many stores and homes.
- 37 • Convenient access to transit service and bike routes.

38 Residences on building levels two and three: Presently, both the PRC and PCD
39 regulations prohibit residences below the fourth floor of a building. However, for
40 structural and financial reasons, mixed-use development is typically configured as
41 dwellings on all building levels above the first floor (street level). Retail or office space at
42 street level is financially viable and appropriate for neighborhoods where pedestrians
43 exist or are desired.

1 There is virtually no market demand for commercial or office space on building levels
2 two and three. Staff supports the prohibition of residences upon the first floor (street
3 level), but is unaware of any other community using this prohibition of dwellings upon
4 building levels two and three, and urges repeal of this development regulation.

5 Proportion of building floor area used for residences: The existing regulations do not
6 allow building's residential floor area to exceed the amount of commercial floor area.
7 For reasons described above, this limitation is inconsistent with standard construction
8 and financing practices for mixed-use. For example, the total floor area of a six-story,
9 mixed-use building would normally be 83 percent residential (floors 2 through 6) and 17
10 percent commercial (first floor only).

11 Maximum lot coverage: Presently, both the PRC and the PCD regulations limit
12 maximum lot coverage at 35 percent. The draft ordinance contains a proposal to
13 eliminate this limitation (for these two zones). The amendment would apply to all land
14 uses and types of development in these two zones.

15 This regulation limits the amount of a development site that may be covered by
16 structure(s). A 35-percent lot coverage requirement creates suburban-scale rather than
17 urban-scale development. This limitation results in an environment where individual
18 buildings are isolated and surrounded by open parking areas. Where off-street parking
19 requirements are met through surface parking, a building can never contain more floor
20 area than can be accommodated by the property's parking lot. As a result, buildings
21 remain relatively small (relative to lot area) and do not achieve densities that can be
22 served efficiently by transit. Smaller-scale buildings contain fewer occupants, and
23 walking distances increase between buildings, and between buildings and transit stops.

24 Limiting development potential through the 35-percent lot coverage requirement has the
25 potential to result in under-development of property—relative to land value. As
26 evidenced by the Lynnwood Place development proposal (where there is no maximum
27 lot coverage requirement), land values in this area of Lynnwood already support
28 privately-financed, structured parking.

29 In order to create a compact, walkable community, at least a portion of off-street parking
30 must be contained within parking structures, so that buildings with shops, offices, and
31 dwellings can be located near one another, which in turn facilitates transit ridership and
32 other modes of transportation. When off-street parking is located within a structure
33 (instead of a surface parking lot) the percentage of land covered by building increases to
34 roughly 80-90 percent. In these instances, areas not covered by a structure are those
35 areas used for landscaping, open space, courtyards, plazas, walkways, etc.

36 Onsite Recreation Area/Open Space: While preparing the draft regulations for onsite
37 open space/recreation area, staff surveyed the regulations of several jurisdictions. The
38 regulations used in other communities vary significantly. The proposed regulations for
39 recreation area/open space (prepared by staff and the Planning Commission) are
40 intended to give project designers flexibility in how onsite recreation area is provided.
41 The draft provisions do not contain a high level of specificity with regards to amenities
42 and improvements for required recreation area/open space, but do ensure new mixed-
43 use development includes at least a modicum of recreation area.

44 As reviewed previously by the Planning Commission, the draft requirements for onsite
45 recreation area/open space can be summarized as follows:

- 1 1. Cumulatively, a minimum of 40 square feet of recreation area per dwelling would
2 be required.
- 3
- 4 2. Developments must include at least two of the following:
 - 5 a. Individual decks, balconies, patios (not less than 6 feet by 6 feet).
 - 6 b. Shared outdoor recreation area (not less than 15 feet by 15 feet). May be
7 located on the building's roof.
 - 8 c. Shared indoor recreation area (not less than 12 feet by 12 feet).

9 Impacts upon City Center: A concern expressed during an earlier Commission meeting
10 (and subsequently addressed) suggested that allowing for mixed use development
11 around Alderwood could detract from the potential for development occurring in City
12 Center. Staff offered the following response and the concern was subsequently
13 withdrawn. The topic is repeated here for informational purposes.

- 14 • First, the ability of a community to “force” development to occur when and where
15 it is convenient is quite limited. Cities can set the table by adopting supportive
16 plans and land use controls and by building infrastructure. However, developers
17 will continue to make investments only when and where they believe they can
18 achieve an adequate return. If a developer wanted to take advantage of the
19 proposed amendment and build a project around the Mall was told that the City
20 wants their project but somewhere else, they could well look elsewhere outside
21 of Lynnwood for sites that meet their expectations.
- 22 • Secondly, significant development in the City Center is beginning to occur
23 because of: macro-economic conditions; development activity elsewhere in our
24 region; and Lynnwood's targeted efforts to attract new investment to the City
25 Center. Staff is already starting to see significant interest in building in City
26 Center, along Hwy 99 and around the Mall, all at the same time and we are
27 hopeful that this will continue. We also note that City Center will soon start to
28 see the benefit of the extension of Light Rail to Lynnwood which will act as a
29 further incentive to build nearby.

30 Traffic impacts: It is expected that multifamily dwellings will generate less traffic during
31 the p.m. peak period than a comparable area of office space. According to the Institute
32 of Transportation Engineers (ITE), general office uses are expected to generate 1.49
33 p.m. peak hour trips per 1,000 square feet of building area. Mid-rise apartment buildings
34 are expected to generate 0.44 p.m. peak hour trips per dwelling. Many apartments are
35 1,000 square feet or larger. This means that allowing dwellings in lieu of offices has the
36 potential to partially mitigate traffic impacts that would otherwise occur.

37 Building height: The proposed amendments will not increase maximum allowed building
38 height. Currently, neither zone has a maximum height limit. The proposed changes
39 affect land uses allowed on the second and third floors, and also would allow a greater
40 percentage of the property to be covered by building(s).

41 Environmental review (SEPA) of new development will occur on a case-by-case basis,
42 and regular mitigation measures will continue to apply (such as traffic impact fees and
43 surface water controls).

1 **Previous Planning Commission / City Council Action**

2 Described above.

3 **Funding**

4 NA.

5 **Key Feature(s)**

6 In the PRC and PCD zones,

- 7 1. The draft ordinance would allow residences within building levels above the
8 first floor. The existing regulations allow residences only upon floors above
9 the third floor.
- 10 2. The draft ordinance would repeal the requirement that residential floor area
11 within a mixed-use building not exceed 50 percent of the total building area.
- 12 3. The draft ordinance would repeal the 35 percent maximum lot coverage
13 requirement for PRC and PCD.
- 14 4. The draft ordinance would establish new requirements for onsite recreation
15 area with new mixed-use development in the commercial zones.

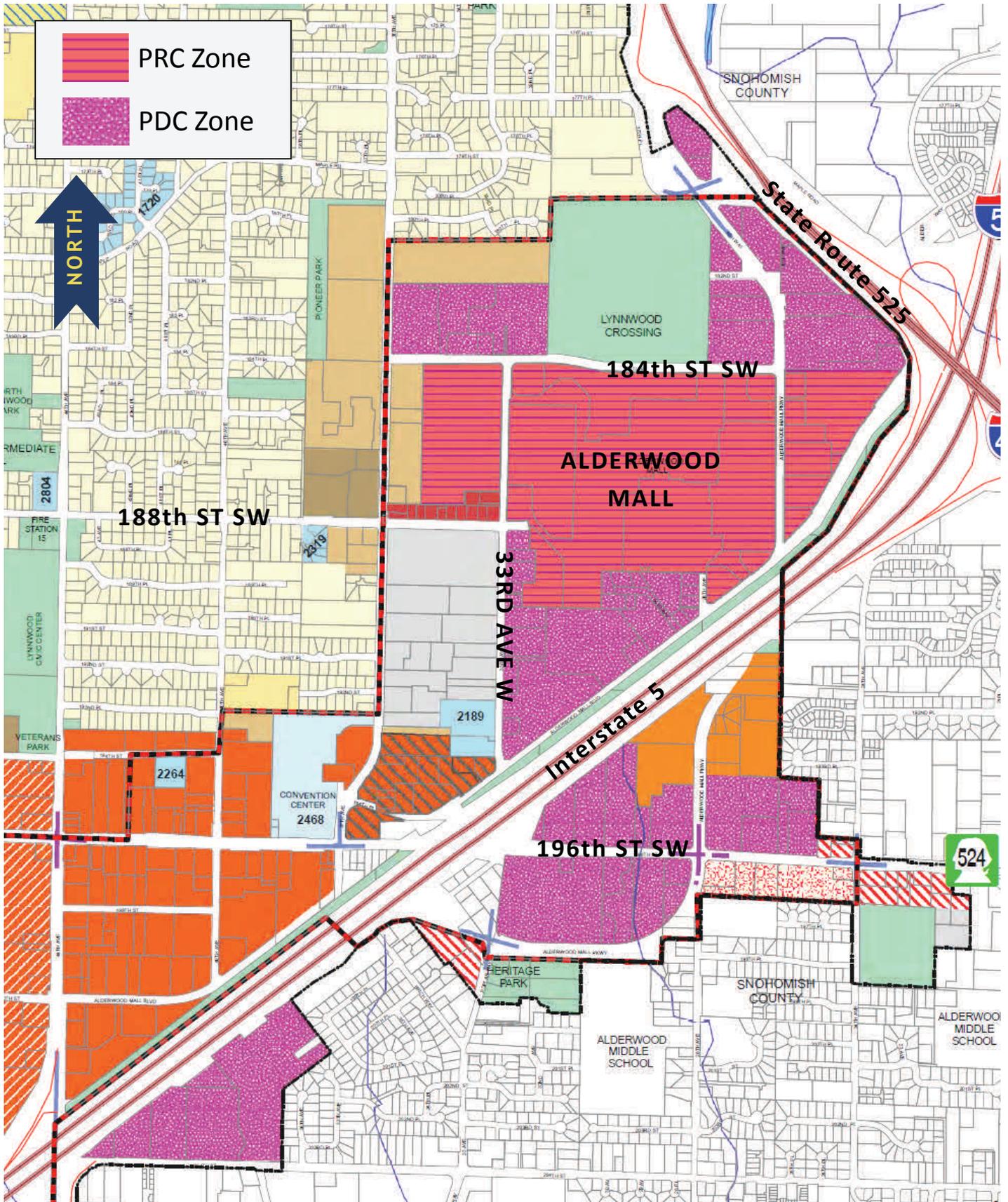
16 **Adm. Recommendation**

17 Receive public testimony during the public hearing and, unless there is compelling
18 testimony to the contrary, recommend the City Council adopt the draft code amendment
19 as written.

20 **Attachments**

- 21 1. Zoning Map excerpt
22 2. Photographic images of mixed-use buildings (examples only)
23 3. Draft ordinance - *updated*
24 4. Written public comment
25 5. Meeting minutes

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CURRENT ZONING

RESIDENTIAL	COMMERCIAL	MIXED USE	INDUSTRIAL	--- Lynnwood City Limits
RS-8 - Residential 8400 Sq Ft	B-1 - Community Business	CC-C - City Center Core	BTP - Business/Technical Park	— MUGA
RS-7 - Residential 7200 Sq Ft	B-2 - Limited Business	CC-W - City Center West	LI - Light Industrial	- - - College District Overlay
RS-4 - Residential 4000 Sq Ft	B-3 - Neighborhood Business	CC-N - City Center North		- - - PSRC Regional Center
RML - Multiple Residential Low Density	B-4 - Restricted Business	MU - Mixed Use	OTHER	Gateway Intersections
RMM - Multiple Residential Medium Density	CG - General Commercial	CDM - College District Mixed Use	P1 - Public	— Gateway
RMH - Multiple Residential High Density	PCD - Planned Commercial Development	HMU - Highway 99 Mixed Use	PUD - Planned Unit Development	— Prominent
MHP - Mobile Home Park	PRC - Planned Regional Shopping Center			Page 27

Examples of Mixed Use Buildings
(With Residences at or Near Second Floor)



CITY OF LYNNWOOD

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF LYNNWOOD, WASHINGTON, RELATING TO RESIDENTIAL USES IN, AND DEVELOPMENT STANDARDS FOR, THE PLANNED REGIONAL CENTER (PRC) AND PLANNED COMMERCIAL DEVELOPMENT (PCD) ZONES, AMENDING LYNNWOOD MUNICIPAL CODE (LMC) 21.46.116, LMC 21.46.200, LMC 21.48.116, AND LMC 21.48.200, AND PROVIDING FOR SEVERABILITY, AN EFFECTIVE DATE AND SUMMARY PUBLICATION.

WHEREAS, under Chapters 35A.11 and 35A.63 RCW, the City Council of the City of Lynnwood has the authority to adopt ordinances relating to the use of real property located within the City; and

WHEREAS, from time to time, it is in the public's interest to amend the City's land use and development regulations to ensure those provisions are consistent with and implement the comprehensive plan and support the public's general health, safety, and welfare; and

WHEREAS, certain provisions of Chapters 21.46 and 21.48 LMC relating to permitted land uses and development standards within the Planned Commercial Development (PCD) and Planned Regional Center (PRC) zones unduly restrict multifamily residential uses; and

WHEREAS, allowing multifamily residential uses within mixed-use buildings within the PCD and PRC zones can support and implement the City's objectives of accommodating future growth in areas where adequate public facilities and services are available, creating a customer base for businesses within the commercial zones, and implementing Smart Growth land use planning principles; and

WHEREAS, in developing the textual code amendments contained herein, the Planning Commission determined that it would be in the community's interest to establish standards for onsite open space and recreation area where multifamily dwellings would be constructed upon commercially-zoned property; and

1
2 WHEREAS, on the 26th day of June, 2014, the City of Lynnwood SEPA
3 Responsible Official issued a Determination of Non-Significance (DNS) on the
4 proposal; and

5
6 WHEREAS, on the 7th day of May, 2012, notice of the proposed code
7 amendment was sent to the Washington State Department of Commerce in
8 accordance with RCW 36.70A.106; and

9
10 WHEREAS, on the 24th day of July, 2014, the Lynnwood Planning
11 Commission held a public hearing on proposed amendments to the Lynnwood
12 Municipal Code provided by this ordinance, and all persons wishing to be heard
13 were heard; and

14
15 WHEREAS, following the public testimony portion of the public hearing, the
16 Planning Commission deliberated on the draft legislation and by regular motion
17 voted to recommend that the Lynnwood City Council adopt the amendments to the
18 Lynnwood Municipal Code; and

19
20 WHEREAS, on the _____ day of _____, 2014, the Lynnwood City
21 Council held a public hearing on proposed amendments to the Lynnwood Municipal
22 Code provided by this ordinance, and all persons wishing to be heard were heard;
23 and

24
25 WHEREAS, based upon the decision criteria specified by LMC 21.20.500, the
26 City Council confirms that the amendments contained herein are: a) consistent with
27 the comprehensive plan; and b) substantially related to the public health, safety, or
28 welfare; and c) not contrary to the best interest of the citizens and property owners
29 of the city of Lynnwood; now, therefore:

30
31 THE CITY COUNCIL OF THE CITY OF LYNNWOOD, WASHINGTON, DO
32 ORDAIN AS FOLLOWS:

33
34 **Section 1. Amendment.** LMC 21.46.116 is hereby amended as follows:

35
36 **21.46.116 Limitations on uses – Residential uses.**

37 A. Motels and Motor Hotels. The initial development must contain at least 20 units
38 composed of multiple-unit type buildings and shall provide hotel services, including a
39 main lobby, desk attendant, and room service. When accessory uses providing
40 services for the motor hotel patrons, such as barber, bar, beauty parlor, cleaners,
41 clothing, drugs, pottery, souvenir, tobacco, and travel are included, they shall be
42 primarily oriented internally. Provisions for public functions such as banquets or
43 meetings need not be oriented internally.

1 B. Multiple-Family Housing.

2 1. Except for properties zoned PCD, dwellings ~~Dwelling units~~ may be permitted
3 in commercial or office buildings on the fourth floor or higher, ~~providing~~ provided
4 no more than one-half the floor area of the building (not including basements) is
5 used for residential purposes. All provisions normally applying to high-rise
6 multiple family housing shall apply.

7 2. For properties zoned PCD, dwellings may be permitted in commercial or
8 office buildings on the second floor or higher, provided that the street-facing
9 ground level of the building is occupied by general commercial or office uses. No
10 dwellings are permitted below the second floor.

11 3. For properties subject to the provisions of this chapter, development with
12 multifamily dwellings shall provide a minimum of 40 square feet of onsite
13 recreation area per dwelling. The onsite recreation area shall consist of a
14 minimum of two of the following:

15 a. Individual patio, deck or balcony immediately adjacent to the
16 corresponding dwelling. Individual patios, decks, or balconies shall be
17 designed so that a 6-foot by 6-foot square will fit within.

18 b. Outdoor recreation area accessible to all residents of the development and
19 designed so that a 15-foot by 15-foot square will fit within. Common outdoor
20 recreation areas shall include features such as: landscaped courtyard or
21 plaza; seating; lighting; roof-top garden; children's play structure; and sport
22 court. Outdoor recreation areas may include overhead weather protection,
23 but may not be enclosed. Landscaping required within parking areas shall
24 not be considered outdoor recreation area.

25 c. Indoor recreation space accessible to all residents of the development and
26 designed so that a 12-foot by 12-foot square will fit within. Indoor recreation
27 areas shall include furnishings and fixtures for activities such as: aerobic
28 exercise; children's play; indoor games; sports; hobbies and crafts; and video
29 entertainment.

30 C. Multiple-Family Housing – Highway 99 Corridor in the Community Business (B-1)
31 and General Commercial (CG) zones. Multiple-family housing is permitted in the B-1
32 and CG zones on specified parcels in the Highway 99 corridor as designated on the
33 city of Lynnwood future land use map. Multiple-family residential development may
34 be combined with mixed use development subject to the following bulk
35 requirements:

**Table 21.46.13(a)
Development Level**

Development standard	Sites with residential development of less than 20 dwelling units per acre	Sites with residential development of 20 dwelling units or more per acre
Minimum lot area	None	None
Minimum setbacks*		
Public street	None	None
Interior property lines	None	None
Ground floor residential units+	10 ft.	10 ft.
Minimum sidewalk width along public streets	12 ft.	12 ft.
Maximum lot coverage	35%	None
Maximum building height	50 ft.	90 ft., not to exceed six stories
Minimum dwelling units/acre++	N/A	20 DU/A
Maximum floor-area ratio	1.0	3.0

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* See LMC [21.62.450](#) for development adjacent to a residential zone (Transitional Property Lines).

+ Applies to residential projects only; setback is from all public rights-of-way, internal circulation (vehicle, bicycle, pedestrian), parking areas, or access easement. Alternatively, where vision-obscuring glass is installed, the setback may be eliminated.

++ The minimum number of residential units to qualify for this level shall be calculated using the entire project site. Where residential development is part of redevelopment of one or more parcels, this calculation shall be based only on the portion of the parcel(s) being redeveloped. Fractional portions of a unit are “rounded up” for this calculation.

Buildings within 200 feet of Highway 99 shall be mixed use development with commercial development on the first floor. Phased development may occur on large parcels but the initial development plan is required to illustrate the commercial activity adjacent to Highway 99.

1 Multiple-family development shall comply with the remainder of the development
2 regulations established in Chapter [21.62](#) LMC, Highway 99 Mixed Use Zone, unless
3 otherwise indicated in Chapter [21.62](#) LMC. Stand-alone multiple-family
4 development or mixed use development shall also comply with the Design
5 Guidelines for the Highway 99 Mixed Use zones.

6 Processing of a multiple-family development, including associated mixed use, will be
7 subject to the provisions set forth in Chapter [21.30](#) LMC, Planned Unit Development.

8 D. Convalescent and Nursing Homes and Housing for the Elderly and Physically
9 Disabled. These uses may be allowed by conditional permit.

10 1. Staff Evaluation and Recommendation. Before any conditional use permit for
11 the uses designated in this subsection is considered by the hearing examiner
12 and city council, a joint recommendation concerning development of the land
13 and/or construction of the buildings shall be prepared by the fire and community
14 development departments, specifying the conditions to be applied if approved. If
15 it is concluded that the application for a conditional use permit should be
16 approved, each requirement in the joint recommendation shall be considered and
17 any which are found necessary for the protection of the health, safety, and
18 general welfare of the public shall be made part of the requirements of the
19 conditional use permit. In any case, the approval of the conditional use permit
20 shall include the following requirements:

21 a. The proposal's proximity to stores and services, safety of pedestrian
22 access in the vicinity, access to public transit, design measures to minimize
23 incompatibility between the proposal and surrounding businesses;

24 b. Compliance with all applicable state, federal, and local regulations
25 pertaining to such use, a description of the accommodations, and the number
26 of persons accommodated or cared for, and any structural requirements
27 deemed necessary for such intended use;

28 c. The amount of space around and between buildings shall be subject to the
29 approval of the fire chief as being adequate for reasonable circulation of
30 emergency vehicles or rescue operations and for prevention of conflagration;

31 d. The proposed use will not adversely affect the surrounding area as to
32 present use or character of the future development;

33 e. Restriction to such intended use except by revision through a subsequent
34 conditional use permit;

35 2. Development Standards. Housing for the elderly and physically disabled
36 facilities shall conform to the following criteria:

37 a. Lot area per dwelling unit: 1,000 square feet minimum per unit;

1 b. Passive recreation and/or open space: 200 square feet per unit. In the
2 city's higher density multiple-family zones, developments are required to
3 provide active recreational space to help satisfy a portion of the demand for
4 recreational facilities. Housing for the elderly has a similar need but is of a
5 passive nature. Therefore, passive recreation space and/or open space shall
6 be provided. Up to 50 percent of the requirement may be indoors; provided,
7 that the space is utilized exclusively for passive recreation and/or open space
8 (i.e., arts and crafts rooms, solariums, courtyards). All outdoor recreation
9 and/or open space areas shall be set aside exclusively for such use and shall
10 not include areas held in reserve for parking, as per LMC [21.18.800](#). All open
11 space and/or recreational areas shall be of a permanent nature, and they
12 may be restricted to use by tenants only. The use of private and semi-private
13 patios and balconies in meeting these requirements is not permitted.

14 E. Living Quarters, Homeless Teenage Parents. Living quarters designed for
15 homeless teenage parents and their children are permitted in any commercial zone
16 of the city. For the purposes of this section, "living quarters for homeless teenage
17 parents" is defined to mean a building or buildings occupied for living purposes by
18 not more than eight teenage parents and their children.

19 1. Supervision and Maximum Occupancy. Such living quarters must have an
20 adult supervisor residing therein. The maximum number residing therein at any
21 time shall not exceed 2:1, including parents, children, and adult supervisor(s).

22 2. Development Regulations and Standards. Subdivision and zoning
23 development standards for living quarters for teenage parents shall be the same
24 as for the low density multiple-family residential zone (RML). Such quarter shall
25 be treated as an R occupancy for fire and building codes.

26 3. Expiration.

27 a. Notwithstanding below herein, uses established in accordance with this
28 provision shall be considered lawful permitted uses as provided herein for as
29 long as such use continues to exist. Non-use of any living quarter for
30 teenage parents for more than six months shall be deemed to be abandoned
31 and such use shall lose all right to its legal status.

32 b. Except as provided for above, this subsection shall expire on December
33 15, 1992.

34

Section 2. Amendment. LMC 21.46.200 is amended as follows:

21.46.200 Development standards.

The following standards shall apply to all structures and nonstructural uses in the commercial zones:

A. General Area and Dimensional Standards. No building, structure or land shall be established, erected, enlarged or structurally altered, except in conformance with the following standards and in conformance with the adopted building code and applicable Lynnwood Citywide Design Guidelines, as adopted by reference in LMC [21.25.145](#) (B)(3) (for purposes of determining the required yards along public street, the classification of streets indicated on the comprehensive plan shall apply):

**Table 21.46.14
Minimum Standards**

	B-3	B-2	PCD	B-1	CG
Area (unless adjacent to similar zoned land)	none	1 ac.	none	none	none
Maximum Area	none	none	none	none	none
Front Yard					
Located on a principal arterial	15 ft.	15 ft.	15 ft.	15 ft.	15 ft.
Located on all other streets	15 ft.	15 ft.	15 ft.	15 ft.	15 ft.
Side Yard – Street					
Located on a principal arterial	15 ft.	15 ft.	15 ft.	15 ft.	15 ft.
Located on all other streets	15 ft.	15 ft.	15 ft.	15 ft.	15 ft.
Rear Yard	none*	25 ft.	none*	none*	none*
Maximum Building Height	35 ft.†	none	none	none	none

	B-3	B-2	PCD	B-1	CG
Maximum Lot Coverage	35%	35%	35% <u>none</u>	35%	35%

1 Key:

2 * Except where adjoining a residential zone; see LMC [21.46.220](#) and [21.46.230](#).

3 + Except that the maximum height within 25 feet of a residential zone is 25 feet;
 4 except that multifamily is permitted at three stories or 45 feet (whichever is less)
 5 on parcels that have a minimum of one acre in size, subject to standards and
 6 procedures established in Chapter [21.43](#) LMC for the multiple residential
 7 medium-density zone (RMM). See Figure 21.46.1 for specific permitted locations.

8 B. Exceptions.

- 9 1. *Repealed by Ord. 3010.*
 10 2. *Repealed by Ord. 2295.*

11 **Section 3. Amendment.** LMC 21.48.116 is amended as follows:

12 **21.48.116 Limitations on uses – Residential uses.**

13 A. Motels and Motor Hotels. The initial development must contain at least 20 units
 14 composed of multiple-unit type buildings, and shall provide hotel services, including
 15 a main lobby, desk attendant, and room service. When accessory uses providing
 16 services for the motor hotel patrons, such as barber, bar, beauty parlor, cleaners,
 17 clothing, drugs, pottery, souvenir, tobacco, and travel are included, they shall be
 18 primarily oriented internally. Provisions for public functions such as banquets or
 19 meetings need not be oriented internally.
 20

21 B. Multiple-Family Housing. Dwellings ~~units~~ may be permitted in commercial or
 22 office buildings on the ~~fourth~~ second floor or higher, ~~providing no more than one-half~~
 23 ~~the floor area of the building (not including basements) is used for residential~~
 24 ~~purposes. provided:~~
 25

26
 27 1. The street-facing, ground level of the building is occupied by general
 28 commercial or office uses. No dwellings are permitted below the second floor.
 29 All provisions normally applying to high-rise multiple-family housing shall apply.

30 2. For properties subject to the provisions of this chapter, development with
 31 multifamily dwellings shall provide a minimum of 40 square feet of onsite

1 recreation area per dwelling. The onsite recreation area shall consist of a
2 minimum of two of the following:

3 a. Individual patio, deck or balcony immediately adjacent to the
4 corresponding dwelling. Individual patios, decks, or balconies shall be
5 designed so that a 6-foot by 6-foot square will fit within.

6 b. Outdoor recreation area accessible to all residents of the development and
7 designed so that a 15-foot by 15-foot square will fit within. Common outdoor
8 recreation areas shall include features such as: landscaped courtyard or
9 plaza; seating; lighting; roof-top garden; children's play structure; and sport
10 court. Outdoor recreation areas may include overhead weather protection,
11 but may not be enclosed. Landscaping required within parking areas shall
12 not be considered outdoor recreation area.

13 c. Indoor recreation space accessible to all residents of the development and
14 designed so that a 12-foot by 12-foot square will fit within. Indoor recreation
15 areas shall include furnishings and fixtures for activities such as: aerobic
16 exercise; children's play; indoor games; sports; hobbies and crafts; and video
17 entertainment.

18 C. Convalescent and Nursing Homes and Housing for the Elderly and Physically
19 Disabled. These uses may be allowed by conditional use permit:

20
21 1. Staff Evaluation and Recommendation. Before any conditional use permit for
22 the uses designated in this subsection is considered by the hearing examiner, a
23 joint recommendation concerning development of the land and/or construction of
24 the buildings shall be prepared by the fire and community development
25 departments, specifying the conditions to be applied if approved. If it is
26 concluded that the application for a conditional use permit should be approved,
27 each requirement in the joint recommendation shall be considered and any which
28 are found necessary for protection of the health, safety, and general welfare of
29 the public shall be made part of the requirements of the conditional use permit.
30 In any case, the approval of the conditional use permit shall include the following
31 requirements:

32
33 a. The proposal's proximity to stores and services, safety of pedestrian
34 access in the vicinity, access to public transit, design measures to minimize
35 incompatibility between the proposal and surrounding businesses;

36
37 b. Compliance with all applicable state, federal, and local regulations
38 pertaining to such use, a description of the accommodations and the number

1 of persons accommodated or cared for, and any structural requirements
2 deemed necessary for such intended use;

3
4 c. The amount of space around and between buildings shall be subject to the
5 approval of the fire chief as being adequate for reasonable circulation of
6 emergency vehicles or rescue operations and for prevention of conflagration;

7
8 d. The proposed use will not adversely affect the surrounding area as to
9 present use or character of the future development;

10
11 e. Restriction to such intended use except by revision through a subsequent
12 conditional use permit;

13
14 2. Development Standards. Housing for the elderly and physically disabled
15 facilities shall conform to the following criteria:

16
17 a. Lot area per dwelling unit: 1,000-square-foot minimum per unit;

18
19 b. Passive recreation and/or open space: 200-square-foot minimum per unit.
20 In the city's higher density multiple-family zones, developments are required
21 to provide active recreational space to help satisfy a portion of the demand for
22 recreational facilities. Housing for the elderly has a similar need but is of a
23 passive nature. Therefore, passive recreation space and/or open space shall
24 be provided. Up to 50 percent of the requirement may be indoors; provided,
25 that the space is utilized exclusively for passive recreation or open space
26 (i.e., arts and crafts rooms, solariums, courtyards). All outdoor recreation
27 and/or open space areas shall be set aside exclusively for such use and shall
28 not include areas held in reserve for parking, as per LMC 21.18.800. All open
29 space and/or recreational areas shall be of a permanent nature, and they
30 may be restricted to use by tenants only. The use of private and semi-private
31 patios and balconies in meeting these requirements is not permitted.

32
33 D. Living Quarters, Homeless Teenage Parents. Living quarters designed for
34 homeless teenage parents and their children are permitted in any commercial zone
35 of the city. For the purposes of this section, "living quarters for homeless teenage
36 parents" is defined to mean a building or buildings occupied for living purposes by
37 not more than eight teenage parents and their children.

38
39 1. Supervision and Maximum Occupancy. Such living quarters must have an
40 adult supervisor residing therein. The maximum number residing therein at any
41 one time shall not exceed 21, including parents, children, and adult supervisor(s).

42
43 2. Development Regulations and Standards. Subdivision and zoning
44 development standards for living quarters for teenage parents shall be the same

1 as for the low density multiple-family residential zone (RML). Such quarters shall
2 be treated as an R occupancy for fire and building codes.

3
4 3. Expiration.

5
6 a. Notwithstanding below herein, uses established in accordance with this
7 provision shall be considered lawful permitted uses as provided herein for as
8 long as such use continues to exist. Non-use of any living quarters for
9 teenage parents for more than six months shall be deemed to be abandoned
10 and such use shall lose all right to its legal status.

11
12 b. Except as provided for above, this subsection shall expire on December
13 15, 1992.

14
15 **Section 4. Amendment.** LMC 21.48.200 and section 20 of Ordinance 2020, as
16 amended, are each amended as follows:

17
18 **21.48.200 Development standards.**

19 A. Minimum Setbacks. There shall be a minimum setback for buildings of 15 feet
20 from any public street right-of-way and 50 feet from any property line adjoining an
21 RS or RM zone, with the following exceptions:

22 1. Buildings which are to be used for professional offices, and which do not
23 exceed a height of 25 feet above the average finished grade around the
24 foundation of the building, shall be set back 50 feet from any property line
25 adjoining an RS zone and 25 feet from any property line adjoining an RM zone;
26 and

27 2. See LMC 21.16.320 for sign regulations.

28 B. Maximum Building Height. Building height is not restricted.

29 C. Maximum Lot Coverage by Buildings. ~~The maximum lot coverage by buildings is~~
30 ~~35 percent. Lot coverage is not restricted.~~

31
32 **Section 5. Severability.** If any section, sentence, clause or phrase of this
33 ordinance should be held to be invalid or unconstitutional by a court of competent
34 jurisdiction, such invalidity or unconstitutionality shall not affect the validity or
35 constitutionality of any other section, sentence, clause or phrase of this ordinance.

36
37 **Section 6. Effective Date.** This ordinance or a summary thereof consisting of the
38 title shall be published in the official newspaper of the City, and shall take effect and
39 be in full force five (5) days after publication.

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PASSED BY THE CITY COUNCIL, the _____ day of _____, 2014.

APPROVED:

Nicola Smith, Mayor

ATTEST/AUTHENTICATED:

Lorenzo Hines
Finance Director

APPROVED AS TO FORM:

Rosemary Larson
City Attorney

FILED WITH ADMINISTRATIVE SERVICES: _____
PASSED BY THE CITY COUNCIL: _____
PUBLISHED: _____
EFFECTIVE DATE: _____
ORDINANCE NUMBER: _____

1 On the _____ day of _____, 2014, the City Council of the City of
2 Lynnwood, Washington, passed Ordinance No. _____. A summary of the content
3 of said ordinance, consisting of the title, provides as follows:
4

5 AN ORDINANCE OF THE CITY OF LYNNWOOD,
6 WASHINGTON, RELATING TO RESIDENTIAL USES
7 IN, AND DEVELOPMENT STANDARDS FOR, THE
8 PLANNED REGIONAL CENTER (PRC) AND PLANNED
9 COMMERCIAL DEVELOPMENT (PCD) ZONES,
10 AMENDING LYNNWOOD MUNICIPAL CODE (LMC)
11 21.46.116, LMC 21.46.200, LMC 21.48.116, AND LMC
12 21.48.200, AND PROVIDING FOR SEVERABILITY, AN
13 EFFECTIVE DATE AND SUMMARY PUBLICATION.
14

15 The full text of this Ordinance will be mailed upon request.
16

17 DATED this _____ day of _____, 2014.
18
19
20

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Corbitt Loch
Deputy Director
19100 44th ave W
Lynnwood WA 98046-5008
(425) 670-5000

July 09, 2014

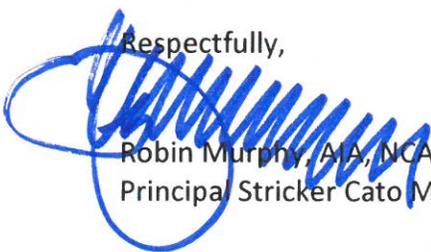
Community Development
Received 7/9/14

Re: Mixed Use in the PRC and PCD Commercial Zones

This letter is in support of the proposed zoning code amendment changes to the PRC and PCD commercial zones. Currently there are many zoning restrictions to mixed use development in the PRC zone. Lynnwood Zoning Code Section 21.48.116 limits multi-family use in this zone. Residential dwelling units may be permitted in office buildings on the fourth floor or higher, providing no more than one-half the floor area of the building (not including basements) is used for residential purposes. Retail use below residential is not clearly defined in the code as well.

We would support cleaning up these restrictions to allow a true mixed use building (residential over commercial use, retail or office) with out regard to where the uses are located in the building envelope. We would also support incentives to increase the current residential density limitations, or number of stories. Other jurisdictions have incentives that allow additional residential DU's based on the developer providing public open space amenities, or through the purchasing of TDRs. The intention of either method is to provide greater density in targeted areas near transit , while also providing an open, more community oriented project. We appreciate you consideration of these suggestions.

Respectfully,



Robin Murphy, AIA, NCARB, LEED AP
Principal Stricker Cato Murphy Architects



Community Development
Received 7/9/14

Mr. Paul M. Krauss
Community Development Director
City of Lynnwood
(425) 670-5406

July 08, 2014

Re: *Mixed Use in the PRC and PCD Commercial Zones*

Dear Mr. Krauss:

We represent Alderwood Professional Building LLC the owner of the property located at 18631 Alderwood Mall Parkway. This property is located within and will be affected by the City's proposed zoning changes.

This letter is provided in full support of the proposed changes to the PRC and PCD commercial zones. Currently there are many zoning restrictions to mixed use development in the PRC zone that hinders the evolution of a vibrant Lynnwood downtown. Specifically Lynnwood Zoning Code Section 21.48.116 limits multi-family use in this zone. Residential dwelling units are permitted in office buildings on the fourth floor or higher, providing no more than one-half the floor area of the building (not including basements) is used for these residential purposes. Retail use below residential is not clearly defined in the code. It is our professional opinion based upon over 30 years experience in urban redevelopment that retail, office, and residential uses all need to intermix and be in close walking proximity to each other in order to create urbane livable communities.

We support removing current restrictions to true mixed use buildings (residential over commercial use, retail and/or office) without regard to where the uses are located in the building envelope. We would also support no setbacks from street ROWs, reduced surface and parking requirements, would further support the inclusion of added incentives to increase proposed residential density limitations, and the number of stories. Neighboring jurisdictions of Seattle, Bellevue, and Redmond in which we have extensive experience have incentives that allow increased densities based on providing public open space amenities, bonus points, and/or through Transfer of Development Rights. The intention of these incentives is to provide greater density in targeted core areas near transit, services, and/or public amenities. Projects that have multiple zoning and land use incentives to choose from have a greater potential of becoming quality community assets.

We appreciate the opportunity to comment on the proposed zoning changes and look forward to providing public testimony at the upcoming public meeting on July 24th.

Thank you.

Oscar Del Moro
Executive Vice President
Cosmos Development Company
Authorized Representative/Alderwood Professional Building LLC

Cosmos Development Company

Cosmos International Center
11747 NE First Street Suite 300
Bellevue, WA 98005
Phone: 425/451-8188
FAX: 425/451-8498



EDMONDS SCHOOL DISTRICT

20420 68th Ave. W., Lynnwood, WA 98036-7400

<http://www.edmonds.wednet.edu>

Stewart Mhyre, MBA, CSBA
Executive Director
Business & Operations

Includes Brier, Edmonds, Lynnwood, Mountlake Terrace, and Woodway

July 8, 2014

Delivered Via Email

Mr. Corbitt Loch, Deputy Director of Community Development
City of Lynnwood
4114 198th St. SW
Suite 7
Lynnwood, WA 98036

Community
Development Dept.
Received 7/8/14

Subject: Mixed Use in PRC and PCD Commercial Zones (File No.12CAM0004/ERC 001820-2014)

Dear Mr. Loch

Edmonds School District #15 supports the zoning code modification of multi-family building requirements you are proposing for the PRC and PCD zoning in the vicinity of Alderwood Mall. The District owns PCD zoned property at 2927 Alderwood Mall Boulevard in Lynnwood currently operated as its Bus Maintenance and Transportation Facility.

We believe the proposed amendments will allow redevelopment of the District's Maintenance and Transportation site in a consistent manner with surrounding uses and the anticipated market interest for multi-family development in the area. The District previously requested proposals for redevelopment of the site and, based on inquiries for the project it appears there is considerable interest to include a multi-family segment in a planned commercial redevelopment of the property. It appears these code changes would allow a true mixed use approach to redevelopment for the property.

Our recent school bond capital facilities approval includes funding of a replacement maintenance and transportation site away from this heavily developed commercial area. Our current plans would allow construction to begin as early as May 2015. Once under construction, the District could begin marketing the current site for redevelopment. The adoption of these code amendments should help get a positive response from the development community.

We strongly urge the council to approve this legislation as early as possible. If you have any questions or would like to discuss our interest, please call me at 425-431-7015.

Sincerely,

A handwritten signature in black ink that reads 'Stewart Mhyre'.

Stewart Mhyre, Executive Director-Business & Operations

CC: Nick Brossoit, Ed. D - Superintendent

February 12, 2012

Delivered Via Email

Lynnwood Planning Commission
c/o
Mr. Paul Krauss, Director of Community Development
City of Lynnwood
4114 198th St. SW
Suite 7
Lynnwood, WA 98036

Subject: Request to include PCD zone in its current PRC zone evaluation of multi-family uses

Dear Members of the Lynnwood Planning Commission,

Edmonds School District #15 requests your consideration of including the District's property at 2927 Alderwood Mall Boulevard, Lynnwood, WA 98036 in the modification of multi-family building requirements you are considering for the PRC zoning adjoining our property to the north.

We encourage you to consider incorporating a change that would modify the multi-family housing restriction currently in place to allow for multi-family in a mixed use development above a first floor commercial uses. The current requirement calls for allowing multi-family only in office buildings above the fourth floor. This requirement is really more geared for mid-rise or high rise construction. In addition, we would request repeal of the limitation currently in place that multi-family units not exceed 50% of the total building area.

This is important for the PCD zone and in particular this property because of the need to redevelop the District's current Maintenance and Transportation site in a consistent manner with surrounding uses and the anticipated market interest for multi-family development in the area. The District is currently requesting proposals for redevelopment of the site and, based on inquiries for the project it appears there is considerable interest to include a multi-family segment in a planned commercial redevelopment of the property. This change would allow a true mixed use approach to redevelopment.

We believe the property could be more attractive for developers if the current multi-family restrictions were relaxed. The District has met with staff previously regarding this topic and was encouraged to make this request.

Thank you for consideration of this request. If you have any questions or would like to discuss our request, please call me at 425-431-7015.

Sincerely,

Stewart Mhyre, Executive Director-Business & Operations

Mixed Use in PRC and PCD: Compendium of Meeting Minutes

Excerpt: City Council Meeting Minutes – March 31, 2013 Work Session

Briefing: Regulations for Mixed Use in PRC and PCD Zones 30

- Staff (Krauss, Loch & Kleitsch) participated. Director Krauss opened with a brief history and 1 background.
- Deputy Director Loch delivered a report on the proposed changes. 33
- Council comments and questions. 34
- It was the consensus of the Council to move forward with Policy Question #1 Should the existing 35 regulations for mixed use in the PRC and PCD zones be amended? Yes.

Excerpt: Planning Commission Meeting Minutes - March 28, 2013 Work Session

3. MIXED USE REGULATIONS FOR THE PLANNED REGIONAL SHOPPING CENTER AND PLANNED COMMERCIAL DEVELOPMENT ZONES

Continued discussion of proposed amendment of land use regulations relating to residential land uses within commercial zones.

Deputy Director Corbitt Loch stated that this item has been discussed by Planning Commission a few times, and each time there has been refinement and improvement to the legislation. This is another step in the process of amending the Planned Regional Center (PRC) zone and the Planned Commercial Development (PCD) zone. The amendment is an initiative to allow multifamily dwellings to be located on the second and third floors of mixed-use buildings in those two zones. He requested the Planning Commission consider the draft ordinance before them and either direct staff to schedule a public hearing, or to make further revisions for the Commission's review. The proposed amendments to the code are summarized as follows:

- Repeal prohibition of multifamily dwellings on the second and third building levels.
- Repeal the 35% maximum lot coverage provision for the PRC and PCD zone.
- Add requirements for onsite recreation area/open space for mixed use in commercial zones regulated by Chapter 21.46 LMC.

With regard to the recreation requirements, staff has taken an effort to provide standards that would be compatible with the urban environment and would not discourage or preclude development of property. Comparisons with other jurisdictions were included in the packet. The proposed standards state that overall there be a minimum of 40 square feet of open space or recreation area per dwelling. There are many different options available to the developer for achieving this, but they must include at least two of the following: a) individual decks and balconies; b) shared outdoor recreation area; or c) shared indoor recreation area.

Commissioner Larsen pointed out that there have been three agenda items tonight that promote multifamily development. He expressed concern that condominiums are not included as an option and commented on the value of home ownership for the community. He said he would like to know how the law works on condos and that he'd like to have more discussion about whether or not the City wants to support and promote home ownership. He also would like to

have more discussion about the amount of multifamily development proposed for the City. He commented that Alderwood Mall and the new Lynnwood Place development constitute a community center and will attract new development in the area. He is proud of living here and being a part of this, but would like to further discuss the relationship of ownership versus rental to determine if there are any issues or ramifications they need to be aware.

Director Krauss acknowledged that every community has a desire to promote ownership because it implies an investment in the community and less turnover. However there is no mechanism that he is aware of that allows the City to mandate it. Regarding condominiums, he explained that condo development ran into many liability issues related to the type of construction, the ownership, and state law, but he thought that the legislature amended some things recently to address that issue. He noted that there are some condo developments happening in the community.

He referred to what is being proposed by a developer near the mall and emphasized that the exact same building is already permitted by City code. The only differences are that Lynnwood has a prohibition against putting the residential uses on the second and third floors. The other change that is being advocated is an increase in the amount of land that can be occupied by structures. Staff thinks that this makes sense not only for mixed use development, but for the mall itself. The 35% maximum lot coverage is a suburban standard and means that you will always have large seas of asphalt surrounding buildings for parking. Director Krauss said he doesn't think the changes themselves are that dramatic, and they remove potential roadblocks for new development.

Commissioner Braithwaite raised a concern about the conversion of property from commercial to residential uses and its impact on tax generation for the City. Director Krauss pointed out that there would still a requirement that the ground floor be commercial so there still will be that commercial component. Lynnwood Place has first-floor commercial with residential above it. It also has a few exclusive commercial buildings plus Costco so it is a mix of commercial and residential.

Commissioners asked about national and regional trends regarding commercial development. Director Krauss stated that what they are seeing in the region and around the country is the transitioning of commercial space to other uses. Alderwood, fortunately, is one of the winners in this aspect. However, there are only so many big boxes out there. The demand has also changed as a result of the recession. Before the economy tanked, General Growth approached him and David Kleitsch to see if the City might be receptive to putting their parking into more parking garages and freeing up space for offices, housing, and more retail pads. Staff was excited about the notion, but never got a chance to take it before the Council due to the recession and General Growth going through bankruptcy. He is confident, however, that that sort of thing will happen again. Staff sees Alderwood Mall retaining or even increasing its importance as a regional center, but also maturing to a model that's a little more intense than the sprawling suburban model. Deputy Director Loch added that with online shopping people can easily buy things from home. What staff is hearing is that people go out to go shopping for an experience, which is why retail centers like University Village are popular.

Commissioner Wojack spoke in support of the proposed minimum dimensions for decks. He asked how easy it would be to convert apartments to condos. Director Krauss discussed the history of condo conversions. While there are a lot of reasons to encourage the permanence of ownership, there is also some value to striving to have more upward economic diversity as well. He commented on the high quality of the proposed multifamily development within Lynnwood

Place, and the value those residents provide to the community. He noted that staff has been working with a developer who is bringing in a 300-unit senior building in City Center. City Center needs people to live in it to be the customers of the stores that we want to see.

Commissioner Larsen commented that the other side of his thinking on multifamily is that it is highly likely that these kinds of commercial centers will begin to take on a life and energy of their own. He discussed high-density areas of Germany where he spent some time when he was younger which were very exciting. He thinks this is probably Lynnwood's future, and we have a chance to do a very good job with it.

Director Krauss stated that one of the primary reasons why Lynnwood did things like the Highway 99 area and some of these things around Alderwood Mall and the City Center is because there is an overriding goal of protecting Lynnwood's single-family neighborhoods. While we do have obligations to accept growth under State GMA, Lynnwood has made a conscious decision to not do it at the expense of single-family neighborhoods. Snohomish County made the opposite decision (LDMRs) which has had undesirable results.

Motion made and seconded to schedule a public hearing for this draft legislation. Motion passed unanimously.

**Excerpt: Planning Commission Meeting Minutes - February 28, 2013
Work Session**

2. Mixed Use Regulations for the Planned Regional Center (PRC) and Planned Commercial Development (PCD) Zones

Continued discussion of potential amendment of land use regulations relating to residential land uses.

Deputy Director Loch stated that this was continued review by the Planning Commission of draft changes to the two zones near the mall. One is the Planned Regional Center (PRC) zone. The other is the Planned Commercial Development (PCD) zone. Staff has made edits as directed by the Commission at the last meeting and brought them back for another review. He solicited comments regarding this item. Ultimately, staff hopes to have a public hearing before the Planning Commission and then another hearing before the City Council.

Deputy Director Loch explained that the main emphasis of the amendments involved the location of multifamily dwellings within structures that also have office or retail in them. Presently the zoning code does not allow residences below the fourth floor. This code amendment would relax that standard by allowing dwellings above the first floor. He stressed that the proposed amendments do not dramatically allow different land uses in the area. Another code amendment would relax the requirement that the amount of multifamily building area not exceed 50% of the total building. Staff finds this overly restrictive and recommends that it be repealed altogether. Additionally, staff is recommending that the 35% maximum lot coverage be repealed. To address concerns over potential height issues, staff has included a maximum height limit for new buildings with residences in them of 65 feet if they are within 500 feet of single-family area. There are no maximum building heights now for either of these two zones. Additionally, the proposed amendments would apply to both the PRC and the PCD zones. Regarding the potential for traffic impacts, he did some research and learned that square-foot for square-foot

multifamily land use would generate about 1/3 of the traffic of office or retail which means it would not necessarily result in an increase in traffic.

Commissioner Braithwaite pointed out a typo on line 11 of page 3 of the ordinance. Deputy Director Loch stated that the entire line should be removed. Commissioner Braithwaite noted that when they eliminate the 35% lot coverage it would not change the parking requirements. He wondered if that would force them to do underground parking. Director Krauss agreed that if developers fill more of the space with structures, they would still have the parking requirements, and would have to have the parking contained within them. Commissioner Braithwaite asked if the code changes only apply to new construction or do they also apply to remodels of existing buildings. Director Krauss thought this could also apply to existing buildings, but it is such a significant change that it would probably only be practical for new construction.

Commissioner Wojack asked what recreation or open space provisions are required for the 500 units at the proposed Lynnwood Place, where there will likely be many families. He encouraged everyone to think about livability for these developments and consider where the kids are going to go to play and hang out. Director Krauss thought that there was a requirement for recreation space for multifamily elsewhere in the City that they could look at adding. He noted that Lynnwood Place is not completely designed yet. That will occur in Cycle 2. There will be open space and recreation space, but it is not known yet how much or what type. One of the things that came up in the discussions was green roofs as private open space for the residents. There is no park requirement as such for the development.

Director Krauss asked whether the Commission wanted staff to make the recommended changes and bring it back one more time before a public hearing. Chair Wright commented that he would be more comfortable with staff bringing this back prior to a hearing to allow additional review time and also to allow the absent commissioners time to add their insight. Commissioner Ambalada strongly encouraged all of the commissioners to attend the next meeting. Director Krauss indicated that staff would fine-tune it and bring it back one more time before the public hearing. He was also hopeful they might have another planning commissioner by the next meeting. Commissioner Ambalada recommended putting what they have discussed in writing for the benefit of the other commissioners, including Mr. Brown's letter. Director Krauss indicated staff could do that.

**Excerpt: Planning Commission Meeting Minutes - February 14, 2013
Work Session**

1. Planned Regional Shopping Center Zone - Mixed Use Regulations, Continued discussion of potential amendment of land use regulations relating to multifamily dwellings.

Deputy Director Loch stated that this item was back again for continued discussion about the degree of regulations that should apply to residential uses in the Planned Residential Commercial (PRC) zone. He explained that there have been some new outcomes since this was brought to the Planning Commission previously so staff wanted to bring it back for confirmation and clarification.

One of the changes is that Snohomish County is looking to establish new population and employment growth targets for Lynnwood and other jurisdictions within the county out to the year 2035. Staff does not have the numbers yet, but is certain there would be some increase in

residential population for the City. This is an extension of the term of the growth target from 2025 to 2035. Also, in January 2013, the City Council directed staff to identify areas for population growth located near the mall. Additionally, the planned Lynnwood Place project has a large number of dwellings within it. If that were approved it would influence the area around the mall.

Residential uses are already allowed in the PRC, but the rules that apply are very stringent and make residential development cost prohibitive. Staff has proposed some amendments to the zoning to give more flexibility and more opportunity for property owners to actually realize multifamily development in a mixed use scenario. These are the same changes that were discussed by the Planning Commission in May of 2012.

Staff recommends the following:

- Allow multifamily residences on the second floor and up (instead of prohibiting multifamily on the first, second, and third floors).
- Allow greater than 50% of building area for residential purposes.

Deputy Director Loch discussed a letter from Edmonds School District regarding the bus barn site. This property has a Planned Commercial Development (PCD) zoning. The school district is asking that the same regulations be adjusted equally on both the PCD and the PRC zoning districts. Staff would like to prepare this for a public hearing which would come back to the Planning Commission. Deputy Director Loch solicited comments or questions from the Planning Commission.

Discussion:

Commissioner Braithwaite brought up the economics of multifamily versus single-family residences in terms of the services required for them. He asked how changing this area to multifamily zoning might affect the city's budget in terms of providing services to the residents. Director Krauss replied that there is a misconception that newer multifamily development creates more demand for services than it pays back in taxes. He stated that there is actually a net benefit to the city because the multifamily residents in newer buildings generally require fewer services than single-family homeowners. One of the reasons is that many people in apartments don't have kids so there is minimal impact to schools. Also, the residents are often younger and don't have as many demands on emergency services. There is also not only the direct benefit of property taxes, but there are also more consumers located in a fairly concentrated area. Regardless of these benefits, Director Krauss pointed out that the City is in the business of providing services and they have multiple means of income to do it.

Commissioner Braithwaite asked what size building staff was envisioning. Director Krauss said they don't know at this point what will be proposed. One investment group has talked to them about a building that would be 7 or 8 stories. The school district would like flexibility also. He discussed common building styles that can be seen in Shoreline along Highway 99. Lynnwood Place is proposing 500 apartments that are a mix of one and two bedrooms. Commissioner Braithwaite asked if they are going to be addressing the density or height limits. Director Krauss stated that the current constraints are for a building that doesn't exist and probably won't exist. It's always been theoretically possible to put multifamily buildings in that area. Director Krauss stated that there has not been a height limit in the Alderwood Mall commercial area in all the years it's existed.

Commissioner Ambalada commented on the demand for apartments for career people. She wondered if they could create residences and work areas for professional, business tenants to live with physical fitness areas on the first floor. She thinks this would be a revenue source for the City and would help to complement the mall by creating a positive environment around the mall. Director Krauss commented that home occupations are already allowed in multifamily in the same way they are allowed in a single-family dwelling. He noted that there is a specific live/work development that is occasionally found. These look like townhouses with the lower floor being a retail area and the upper floor being residences. These types of development tend not to be very high density, but more like townhouses. He discussed the mutually supportive synergies between higher density housing and retail commercial. He also discussed the excellent public transportation with Community Transit in that area. In the future, if the voters approve the next phase of Sound Transit up to Everett, there is supposed to be a stop at Alderwood Mall.

Commissioner Ambalada asked about several items the Council had requested be answered in writing. Director Krauss explained that those questions are answered in the packet that has gone back to the City Council.

Commissioner Braithwaite referred to verbiage stating that "Dwellings are not permitted within the ground/street levels of buildings." He suggested that if there are different grades, they might want to clarify that it is the street level facing the major street that they are referring to.

Staff indicated they would bring back a final version of the ordinance prior to moving it forward to a public hearing.

Motion made by Commissioner Braithwaite, seconded by Commissioner Ambalada, to direct staff to modify the amendments, including the purple areas (PRD) and bring it back to the Planning Commission at another work session in the future. Motion passed unanimously.

**Excerpt: Planning Commission Meeting Minutes - May 24, 2012
Work Session**

1. Planned Regional Center zone, mixed use regulations – Proposed amendment of land use regulations for mixed use development within the Planned Regional Center zone. Draft Ordinance.

Deputy Director Corbitt Loch explained that this item was for discussion purposes only tonight. This is the Commission's first review of the topic of multifamily dwellings in the Planned Regional Center (PRC) zone. The proposal is to relax some of the regulations relating to when multifamily development can occur in that zone. Presently the rules essentially preclude any multifamily development within a mixed use building. There is no change proposed to the rule that prohibits multifamily as a single or a primary use of a property. He stated that this matter was prompted by a developer who is interested in doing a mixed use development within this area, but he found that the rules that exist today are not conducive to the project he feels is appropriate.

There are two standards in LMC 21.48.116 that are being proposed for change:

- Presently multifamily dwellings are not allowed below the fourth floor of a mixed use building; however, the second and third floors of a mixed use building are very difficult to

lease to non-residential uses. The proposal would allow multifamily dwellings above the first floor.

- The proposal would repeal the existing requirement that the amount of residential floor area per building not exceed the amount of floor area dedicated to non-residential land uses.

Commissioner Braithwaite asked for clarification about the location of this zone. Deputy Director Loch described this area on the map. Commissioner Braithwaite asked how much density would be allowed under the proposed code. Deputy Director Loch replied if there is an agreement for this concept of relaxing some of the standards for mixed use development in this area, staff would get more specific about which development standards would apply. This is being brought to the Commission only at the conceptual level at this time. Commissioner Braithwaite asked if the Costco site will be asking for mixed uses. Director Krauss said that site will be asking for a rezone to allow a mix of residential and commercial. He discussed other entities' proposals for this area as well. Commissioner Braithwaite commented that there are both pros and cons to having residential in this area.

Commissioner Ambalada said she strongly thinks the City should avoid high-density residential by the mall and instead focus on mixed use buildings in the City Center area and Highway 99 revitalization area. She thinks this would be more acceptable to single-family neighborhood residents.

Commissioner Larsen asked for more details about what the change would mean. He also asked how the increase would affect Costco's EIS. Finally, he said he'd rather see this come through as a docket item. Director Krauss explained that there is no docket going through this year. Regarding the EIS for the LHS site, he explained that it's a standalone EIS and doesn't relate to anything else that might happen in the area. Anything that comes in subsequent to that development has to do its own environmental review. He discussed factors that need to be considered with regard to traffic in a mixed use development where residents can walk many places instead of driving. He commented that Community Transit has started a new route that goes from downtown Edmonds to Alderwood along 196th Street. CT has stated that it is the fastest growing service they've ever had after the SWIFT BRT.

Commissioner Braithwaite said it would be an interesting economic consideration to see whether the desire for multifamily is detrimental to commercial space or whether it is facilitating commercial space as well. He commented that there is an economic difference to the City in terms of tax revenue and sales tax revenue. Director Krauss clarified that as this is proposed it would not occur without there being commercial development on the ground floor. In that sense, it is similar to projects like the Safeway that was built under a condo or apartment building across from Bel-Square and other projects that have been built in the region. Those sorts of projects would not happen if there was not a foundation for commercial use under it. Director Krauss offered to bring Economic Development Director David Kleitsch to a Planning Commission to give an economic development perspective on this. He referred to Northgate as an example of how this can be a win-win relationship for commercial and residential development. Commissioner Braithwaite noted that Lynnwood could learn from some of the aesthetic mistakes made at Northgate as well. Director Krauss was pleased to report that Lynnwood is far advanced, with regard to design standards, over Seattle.

Chair Wright said that he is generally supportive of allowing mixed use where possible specifically for the idea of saving single family residences.

Commissioner Larsen commented on the traffic issues that already exist at the mall, especially around the holidays. He asked for some input from the traffic engineer regarding the Level of Service for this proposal. Director Krauss agreed that there are some traffic issues at the mall especially around the holidays, but noted that some of the information they've gotten from the Lynnwood High School EIS is intriguing, because the loop road that is incorporated into that project goes a long way to fixing the intersection at Alderwood Mall Parkway and 184th. Additionally, the continuation of that project is to do the design for the Poplar Way Bridge overpass over I-5. That would be a straight shot to 33rd Street and the loop road and would create a whole new way of accessing that area that is not proximate to any of the single-family neighborhoods.

Deputy Director Loch indicated that staff would bring this item back with more detail before it is moved to a public hearing.

City Center Streetscape Plan

Agenda Item: E.1

Staff Report

- Public Hearing
- Work Session
- Other Business
- Information
- Miscellaneous

Staff Contact: Janiene Lambert, City Center Program Manager, Office of Economic Development

Summary

The purpose of this agenda item is to present the Draft City Center Streetscape Plan. The Streetscape Plan is an implementation item called out in the adopted City Center Sub-Area Plan. The Commission will receive a briefing from Staff and the consultant KPG, Inc. No action by Planning Commission is required, however comments are encouraged.

Action

Receive briefing and discuss Draft City Center Streetscape Plan.

Background

On March 14, 2005, City Council adopted the City Center Sub-Area Plan (Plan). Included in the Plan are various policy statements regarding implementation, such as Policy *CCUD 2: Establish Streetscape Standards*.

In accordance with this policy, staff has worked with KPG, Inc. to prepare the City Center Streetscape Plan. KPG utilized the City's draft conceptual streetscape plan as a basis for the plan; held two public outreach meetings; provided construction standard details of streetscape components; provided conceptual design for City Center transit shelters; and provided conceptual design of the 44th Ave. W / I-5 underpass, a major City Center gateway. The completion and implementation of the City Center Streetscape Plan will create a more attractive and functional environment supporting the redevelopment objectives for the City Center.

A public open house was held on February 13, 2014 to kick-off the project and receive community suggestions. On July 23, 2014 another public open house was held to review and discuss the Plan.

Previous Planning Commission / City Council Action

On November 25, 2013, City Council authorized contract #2229 with KPG, Inc. to complete the City Center Streetscape Plan.

On February 13, 2014, Staff and KPG held a public open house for which the Planning Commission was notified and public notice regarding Planning Commission attendance was provided.

Adm. Recommendation

Discuss and provide Staff with comments on the City Center Streetscape Plan.

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100% DRAFT



LYNNWOOD
WASHINGTON

CITY CENTER STREETSCAPE PLAN

JUNE, 2014

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ACKNOWLEDGEMENTS

City of Lynnwood
 Mayor Nicola Smith
 Lynnwood City Council
 City Center Work Group

Consultant
 KPG, Inc.

Special thanks to the Lynnwood Arts Commission, Planning Commission, Diversity Commission, Parks and Recreation Board and the Public Facilities District.

PURPOSE OF THIS DOCUMENT

The development of the City Center Streetscape Plan stems from City Center Sub-Area Plan policies 1, 2, 9, and 11 and provides needed details for the implementation of many street projects identified through the City Center project prioritization effort. The Standards in the City Center Streetscape Plan have been established to facilitate and streamline permitting by providing pre-approved plans, assure over time that the City Center streetscape design, and the materials and furnishings used, create a distinct cohesive identity and attractive ambience for the City Center, which will serve as the social, commercial and cultural heart of the community. These highly visible improvements are vital to the success of the City Center, and will provide for the everyday interaction

of people, for community events, and for development of adjoining private property.

The Streetscape Plan is intended to build upon the urban design principles of the City Center Sub-Area Plan, City Center Zoning and the City Center Design Guidelines. The Plan will help to advance the revitalization process, creating safer public spaces and generating a greater sense of community pride and economic vitality. The revitalized streets within the City Center will help attract new residents, businesses, and visitors to the richness of Lynnwood's urban landscape.

HOW TO USE THIS DOCUMENT

CONTENT TBD

INTRODUCTION | WHAT IS A STREETScape?

Streetscape: the appearance or view of a street.

streetscape noun \ˈstrēt-ˌskāp\ Source: merriam-webster.com

In the Lynnwood City Center, this area includes, but is not limited to, the sidewalk surface, medians, crosswalks, street trees, lighting and furnishings. Furnishings can include benches, planters, tree grates, litter and recycling receptacles, bicycle racks, transit shelters, newsstands, bollards and banners. The sidewalk surface generally runs from the building face to the curb and may include public plazas.

DESIGN PRINCIPLES

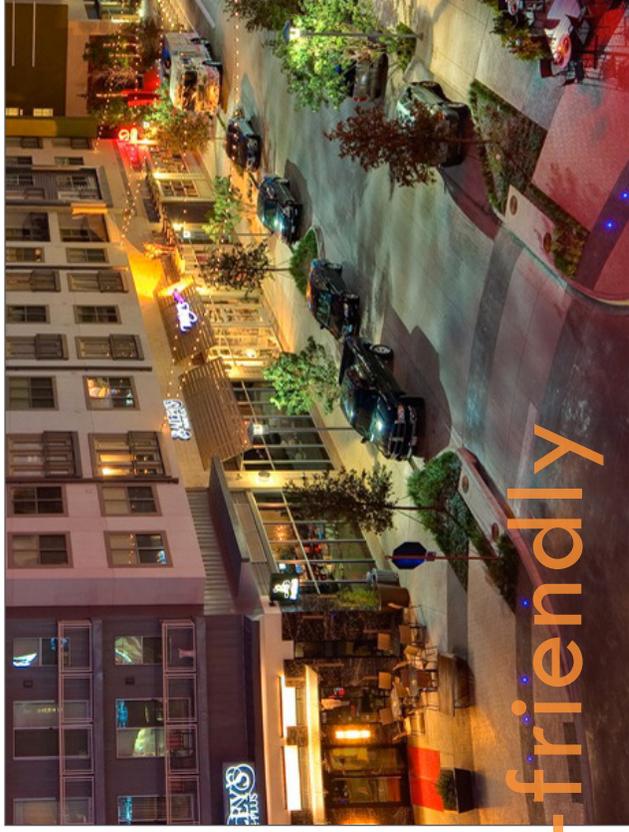
- Attract and support pedestrian activity
- Create character and a sense of place and community
- Humanize the streets
- Design for aesthetics as well as function
- Create an appealing and safe environment for pedestrians
- Utilize sustainable options

DESIGN

The streetscape design approach will embrace a contemporary, urban, pedestrian theme, with durable, low-maintenance materials, following low impact development principles where possible. The Streetscape Plan classifies the streets by design ‘types’ that reflect the character of the area in which they are located. Street overlays address the treatment of gateways, corners and intersections. With careful implementation, the Streetscape Plan will create a unique character for the City Center, providing an attractive, safe and pedestrian-friendly environment.

This document provides guidance for the design and development of the City Center streetscape. The design standards for streetscapes are subject to the Lynnwood Municipal Code, Title 12: 12.1.2.030.

contemporary
unique
urban
pedestrian-friendly



DEFINITIONS

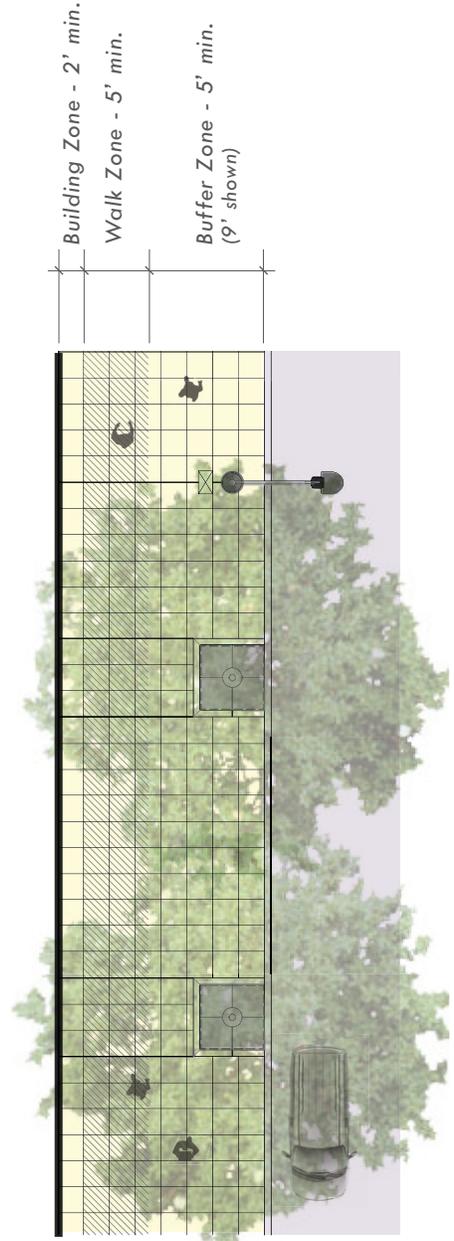
- **Crime Prevention Through Environmental Design (CPTED):** A multi-disciplinary approach to deterring criminal behavior through environmental design. CPTED strategies employ design to create safer spaces.
- **Gateway & Prominent Intersections, Celebratory Corners:** Major intersections and access points into the City Center are identified as either Gateway Intersections, Prominent Intersections or Celebratory Corners. These intersections shall have different features that provide orientation and identify for the City Center.
 1. **Gateway Intersection:** Identify major entrance to the City at 196th Street SW & 36th Avenue W
 2. **Prominent Intersection:** Identify the major arterial intersection at 44th Avenue W & 196th Street SW
 3. **Celebratory Corners:** Identify the major intersections along the Promenade Street (198th Street SW) at 44th Avenue W, 42nd Avenue W and 40th Avenue W
- **Low Impact Development (LID):** A stormwater management strategy that emphasizes on-site management, conservation and use of natural site features integrated with stormwater controls to mimic natural hydrologic patterns in urban settings. Low Impact Development employs principles that preserve and recreate natural landscape features, minimizing impervious surfaces to create site drainage that treats stormwater as a resource rather than a waste product, e.g. bioretention facilities, rain gardens, vegetated rooftops, rain barrels, and permeable pavements.
- **Streetscape:** The streetscape throughout this plan generally refers to the exterior public spaces located between street curbs and building facades (the sidewalks), but also refers to urban roadway design as it impacts street users, both pedestrian and vehicular. Streetscapes can have a significant effect on how people perceive and interact with their community. If they are safe and inviting to pedestrians, people are more likely to walk which helps reduce automobile traffic, improve public health, stimulate local economic activity, and attracts residents and visitors to a community.
- **Streetscape Types:** There are four main types of streetscapes for the public right of ways with different standards and amenities to improve the pedestrian environment. These types mitigate the volume, speed and width of the vehicular traffic and strategically implement place-making and use of the sidewalks spaces to increase pedestrian and social activity. Other streetscape walkways and overlays include the Promenade Walkway, Gateway Intersection, Prominent Intersection and Celebratory Corners.
- **Promenade Street:** Refers to the public pedestrian and vehicular way along 198th St. SW identified in the City Center Sub-Area Plan bounded on the east by 40th Avenue W and on the west by 44th Avenue W unless 198th Street SW is extended west of 44th Avenue in which case the west boundary shall be the westerly terminus of 198th Street SW within the City Center.
- **Promenade Walkway:** Refers to the pedestrian connection conceptually illustrated in the City Center Sub-Area Plan that connects the Promenade Street to the Transit Center on the South and the Convention Center and towards Alderwood Mall on the North.
- **Public Art:** The term “public art” applies to any art (irrespective of ownership) which is exhibited in a public space. Public art can be in the form of painting, mural, mosaic, sculpture, landscaping, or other work of art, as long as it can be appraised as a work of art and its value documented. Public art can be displayed on buildings, at or near pedestrian entrances, sidewalks, in public plazas or parks.

Public art attracts people and humanizes the urban landscape. Art will add character and strengthen connectivity in the City Center, inviting people to explore, moving them from one art piece to another. Lynnwood has an already established public art program, much of which has been funded by the “1% for Arts” program generated by city construction projects. Throughout Lynnwood’s municipal buildings, parks and public spaces, there are 23 site-specific sculptures and a portable collection of over 110 works.

SIDEWALK ZONES

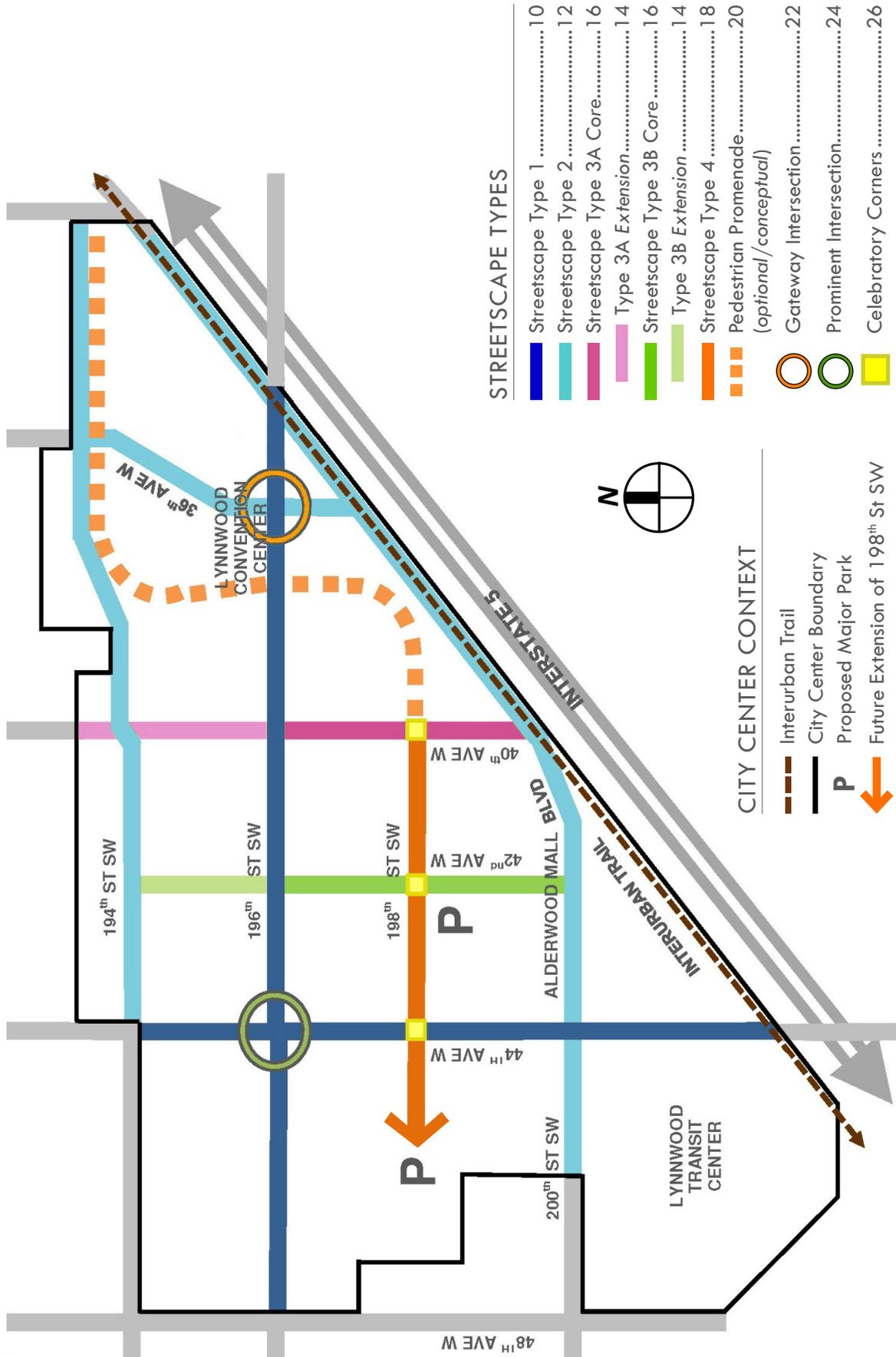
The sidewalk portion of the streetscape shall consist of three main sidewalk zones:

- **Building Zone (2' min. width):** The Building Zone allows for access to building entrances, room for window shopping, and protection from inclement weather through awnings and building overhangs. Café seating and other pedestrian amenities may be appropriate for encroachment into the Building Zone as long as the minimum clear Walk Zone width is maintained. This zone is paved and may also provide space for small portable signage, movable planters or outdoor displays.
- **Walk Zone (5' min. width):** The Walk Zone allows for unobstructed pedestrian movement on the sidewalk between the Buffer Zone and the Building Zone. The Walk Zone should be increased proportionally to the overall sidewalk dimension.
- **Buffer Zone (5' min. width):** The Buffer Zone is located closest to the street and serves as a separation between pedestrians and vehicles. This zone may include a variety of amenities including street trees, lighting, street furniture, planters, utilities, parking meters and public art. Street trees installed in tree grates shall be provided in the Buffer Zone. The width of the Buffer Zone may be increased proportionally to the overall sidewalk dimension to accommodate larger seating areas.
- **Clear Zones at Sidewalk Intersections:** Clear zones are sidewalk areas at intersections where only public fixtures, such as traffic lights, traffic signal equipment (such as signal control cabinets), street signs and signal poles, are permitted. This may not be possible at all intersections but serves as a model to accommodate the high pedestrian volumes that occur at sidewalk intersections. In addition these areas shall provide safe viewing distances for pedestrians as well as motorists. The Clear Zone begins a minimum of 10 feet from the corner of each building facade and extends perpendicular to the curb face.
- **Other Requirements:** Placement of sidewalk amenities must conform to City requirements.



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CITY CENTER STREETScape TYPES



STREETScape TYPES

- Streetscape Type 110
- Streetscape Type 212
- Streetscape Type 3A Core.....16
- Type 3A Extension.....14
- Streetscape Type 3B Core16
- Type 3B Extension14
- Streetscape Type 418
- Pedestrian Promenade.....20 (optional/conceptual)

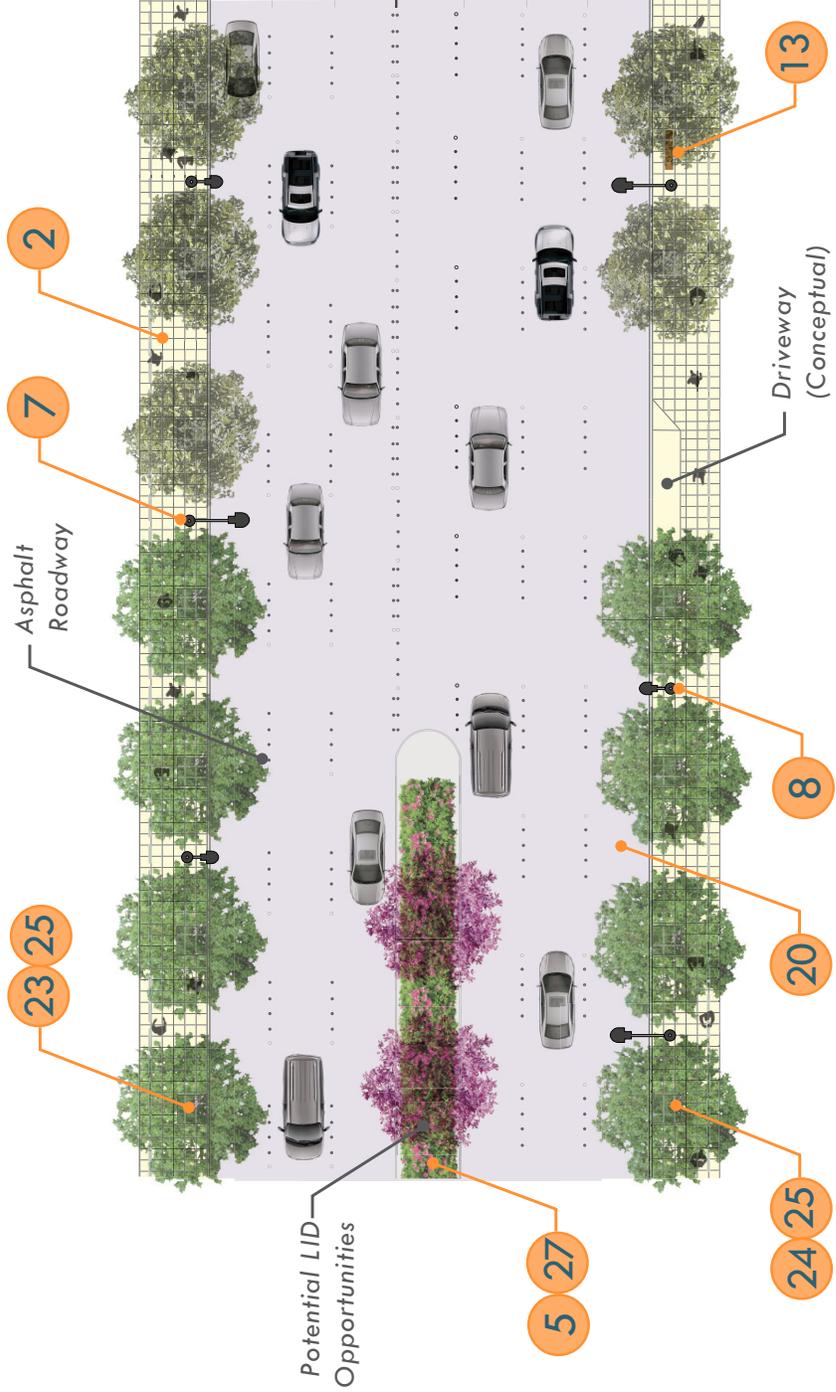
- Gateway Intersection22
- Prominent Intersection.....24
- Celebratory Corners26

CITY CENTER CONTEXT

- Interurban Trail
- City Center Boundary
- P Proposed Major Park
- Future Extension of 198th St SW

STREETSCAPE TYPE 1 REQUIRED ELEMENTS

- 2 Standard Sidewalk Pavement 32
- 5 Planted Roadway Median 33
- 7 Street Lighting, typ. [120' O.C., staggered] 36
- 8 Pedestrian Lighting, typ. [120' O.C., staggered] 36
- 13 Standard Bench [two (2) min. per block w/in 100' of intersection] 46
- 20 Decorative Utility Covers (as required) 54
- 23 Standard Street Tree Grate, typ. [30' O.C., paired] 56
- 24 Panel Street Tree Grate (optional alternate to standard) 57
- 25 Street Tree, typ. [30' O.C., paired] 58
- 27 Shrubs, Grasses, Groundcovers & Perennials 60

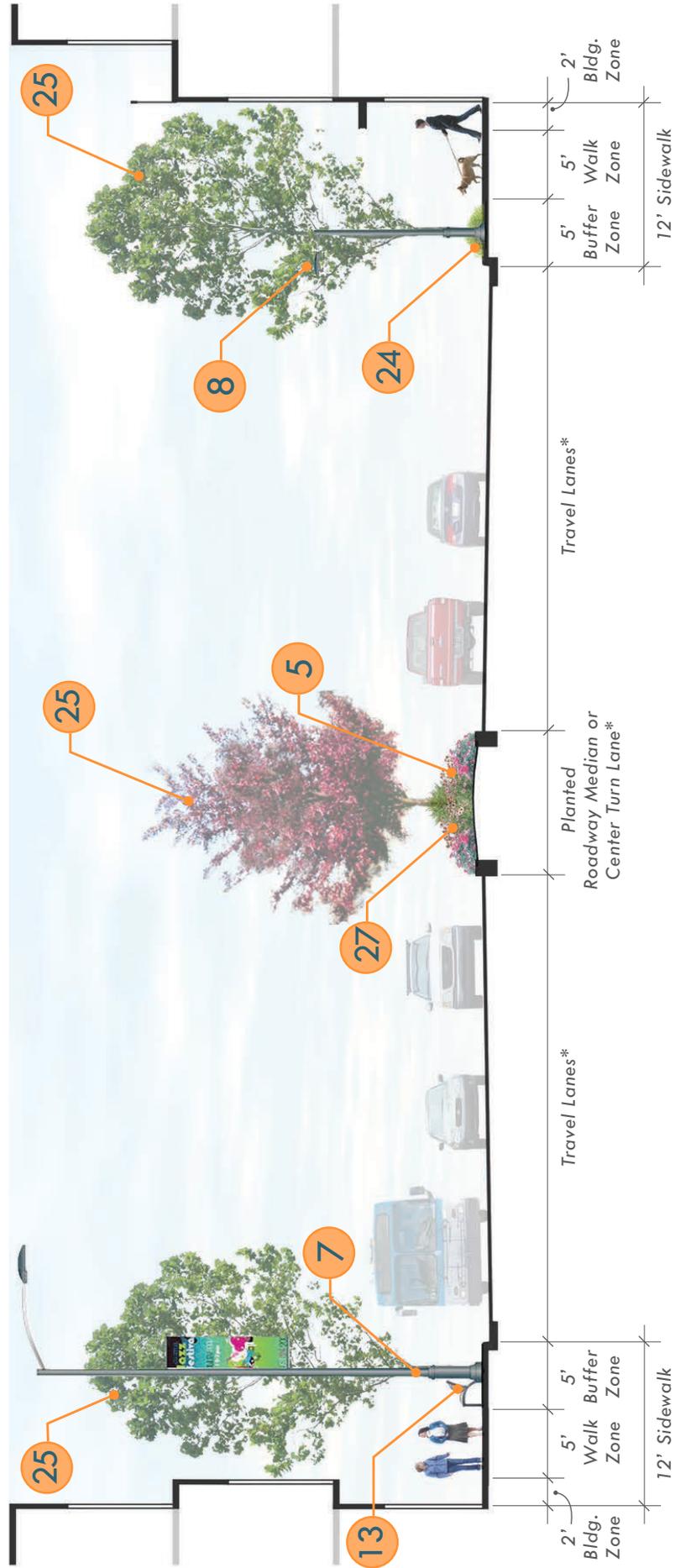


NOTE: Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood

CITY CENTER STREETScape TYPES | STREETScape TYPE 1

44TH AVENUE W • 196TH STREET SW

ADD KEY



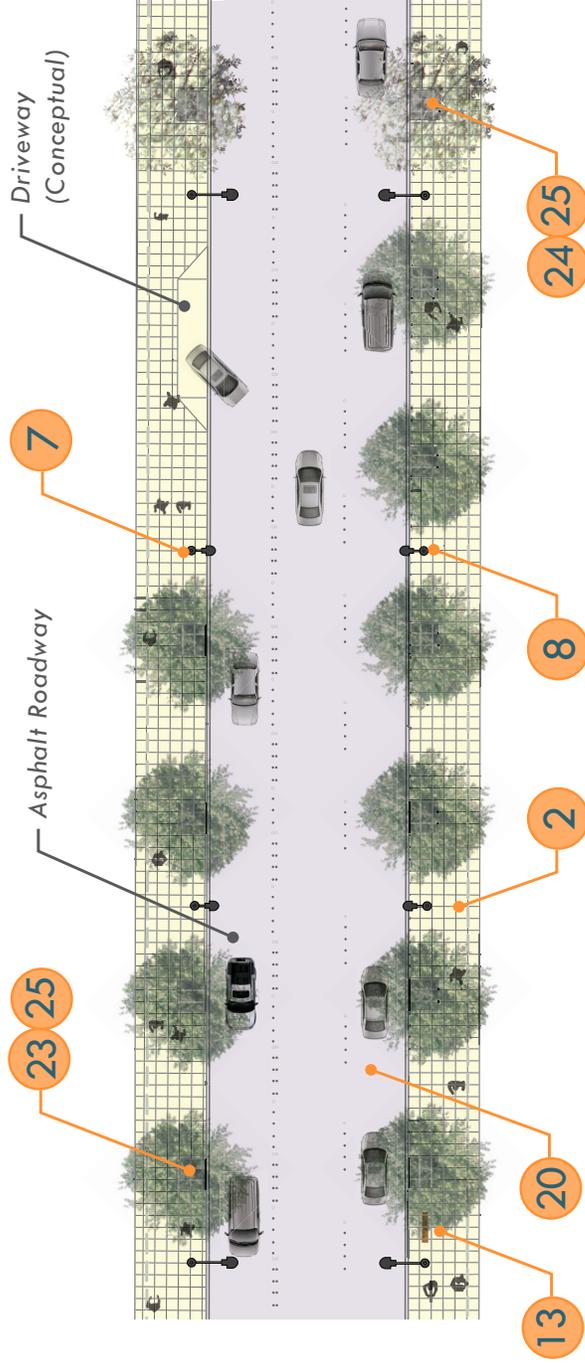
*See Appendix B for specific roadway sections including number of travel lanes, on-street parking requirements and multi-modal facilities.

CITY CENTER STREETSCAPE TYPES | STREETSCAPE TYPE 2

36TH AVENUE W • 194TH STREET SW • 200TH STREET SW • ALDERWOOD MALL BOULEVARD

STREETSCAPE TYPE 2 REQUIRED ELEMENTS

- 2 Standard Sidewalk Pavement 32
- 7 Street Lighting, typ. [180' O.C., paired] 37
- 8 Pedestrian Lighting, typ. [60' O.C., paired]..... 37
- 13 Standard Bench [two (2) min. per block w/in 100' of intersection]..... 46
- 20 Decorative Utility Covers (as required)..... 54
- 23 Standard Street Tree Grate, typ. [30' O.C., paired] 56
- 24 Panel Street Tree Grate (optional alternate to standard) 57
- 25 Street Tree, typ. [30' O.C., paired]..... 58

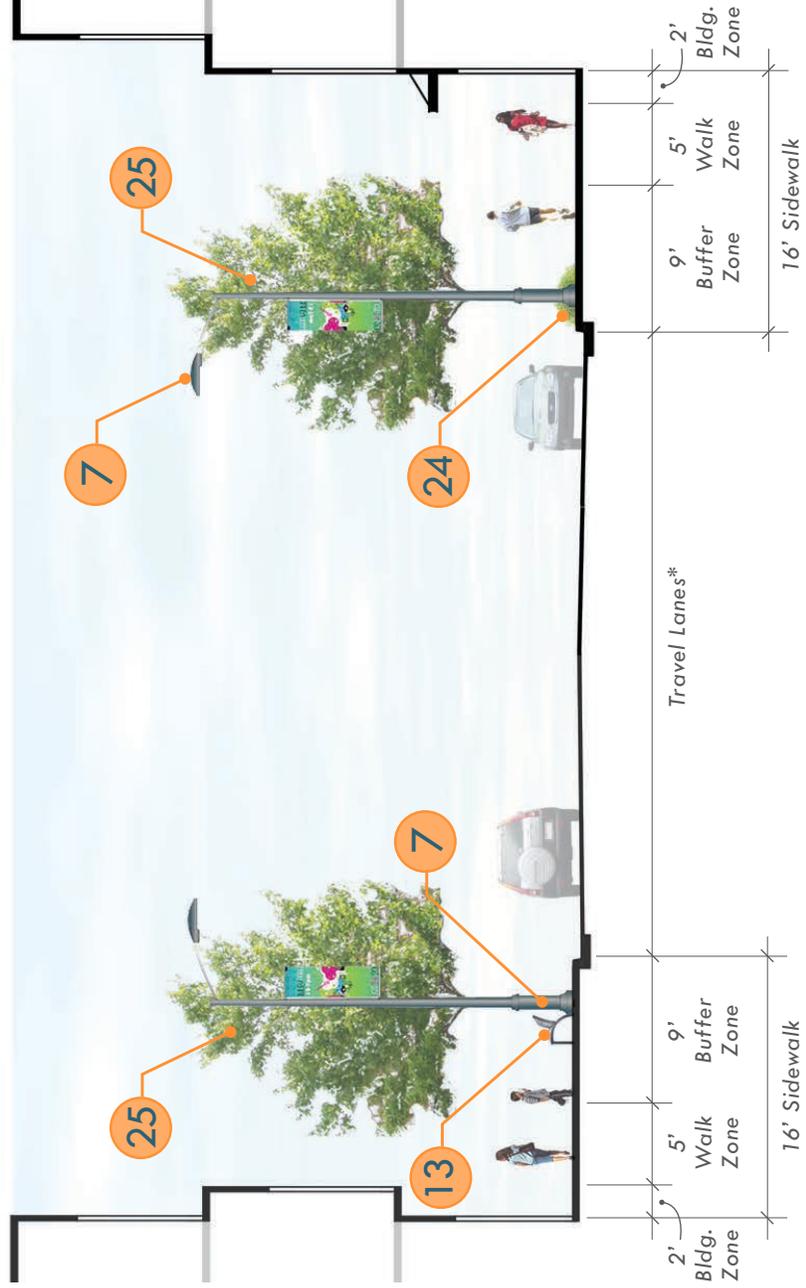


NOTE: Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood

CITY CENTER STREETScape TYPES | STREETScape TYPE 2

36TH AVENUE W • 194TH STREET SW • 200TH STREET SW • ALDERWOOD MALL BOULEVARD

ADD KEY



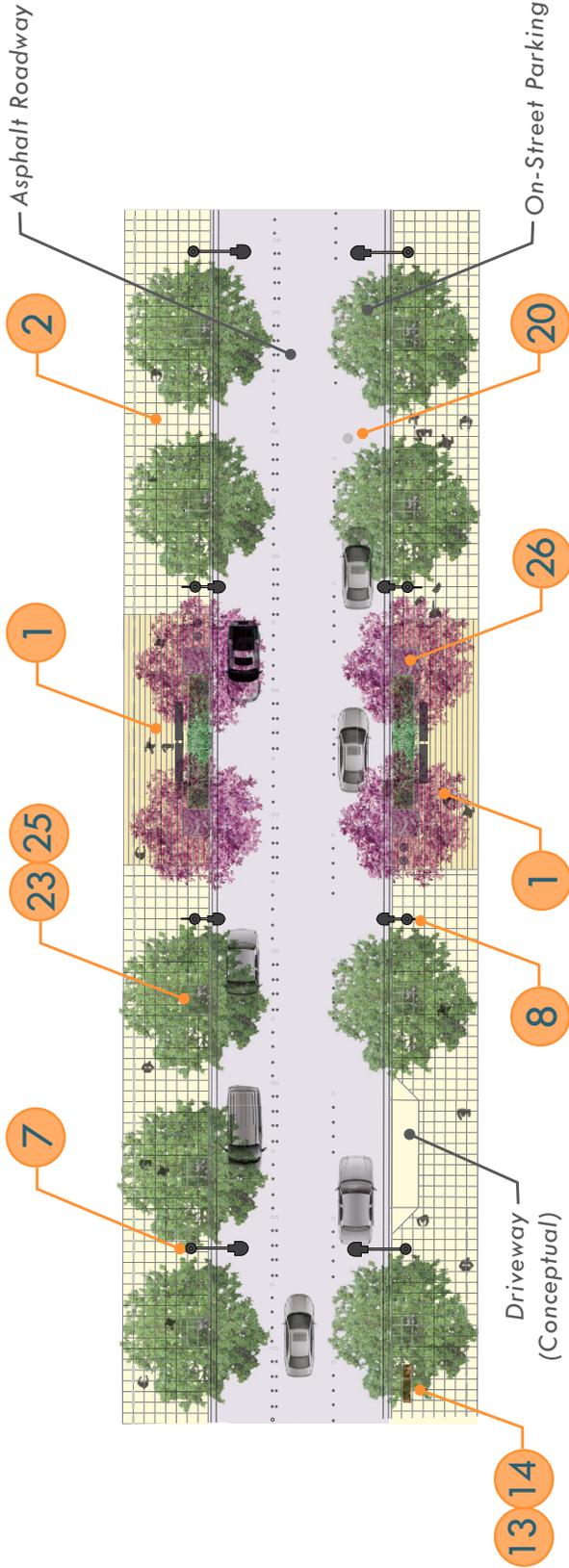
*See Appendix B for specific roadway sections including number of travel lanes, on-street parking requirements and multi-modal facilities.

CITY CENTER STREETSCAPE TYPES | STREETSCAPE TYPES 3A & 3B EXTENSION

40TH AVENUE W • 42ND AVENUE W | FROM 194TH STREET SW TO 196TH STREET SW

STREETSCAPE TYPE 3A REQUIRED ELEMENTS

- 1 Pedestrian Amenity Cluster, typ. 30
- 20 Decorative Utility Covers (as required)..... 54
- 2 Standard Sidewalk Pavement 32
- 23 Standard Street Tree Grate, typ. [26' O.C., paired] 56
- 7 Street Lighting*, typ. [180' O.C., paired] 38
- 25 Street Tree, typ. [26' O.C., paired] 58
- 8 Pedestrian Lighting*, typ. [60' O.C., paired]..... 38-39
- 26 Accent Tree, typ. [20' O.C., paired] 59
- 13 Standard Bench [three (3) min. per block w/in 100' of intersection] ... 46
- 27 Planter (at Pedestrian Amenity Cluster, optional) 60
- 14 Custom Bench (optional alternate to standard) 48

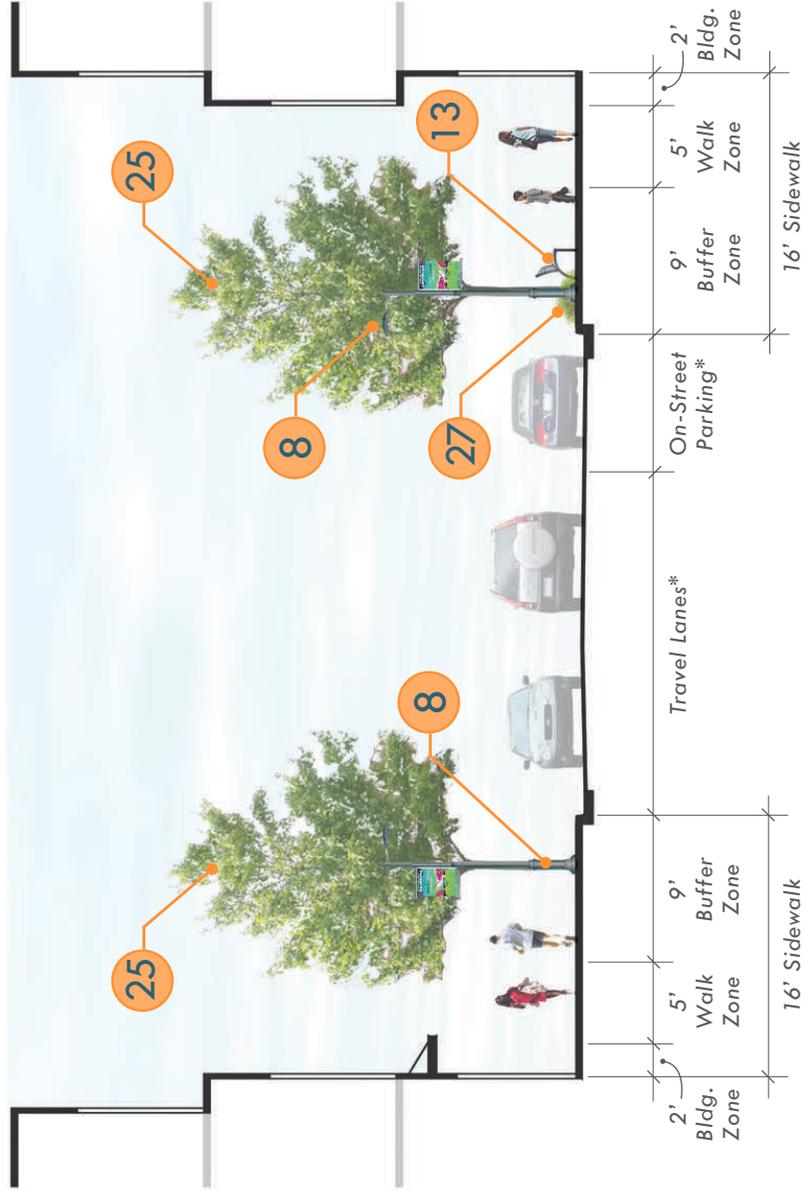


NOTE: Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood

CITY CENTER STREETScape TYPES | STREETScape TYPES 3A & 3B EXTENSION

40TH AVENUE W • 42ND AVENUE W | FROM 194TH STREET SW TO 196TH STREET SW

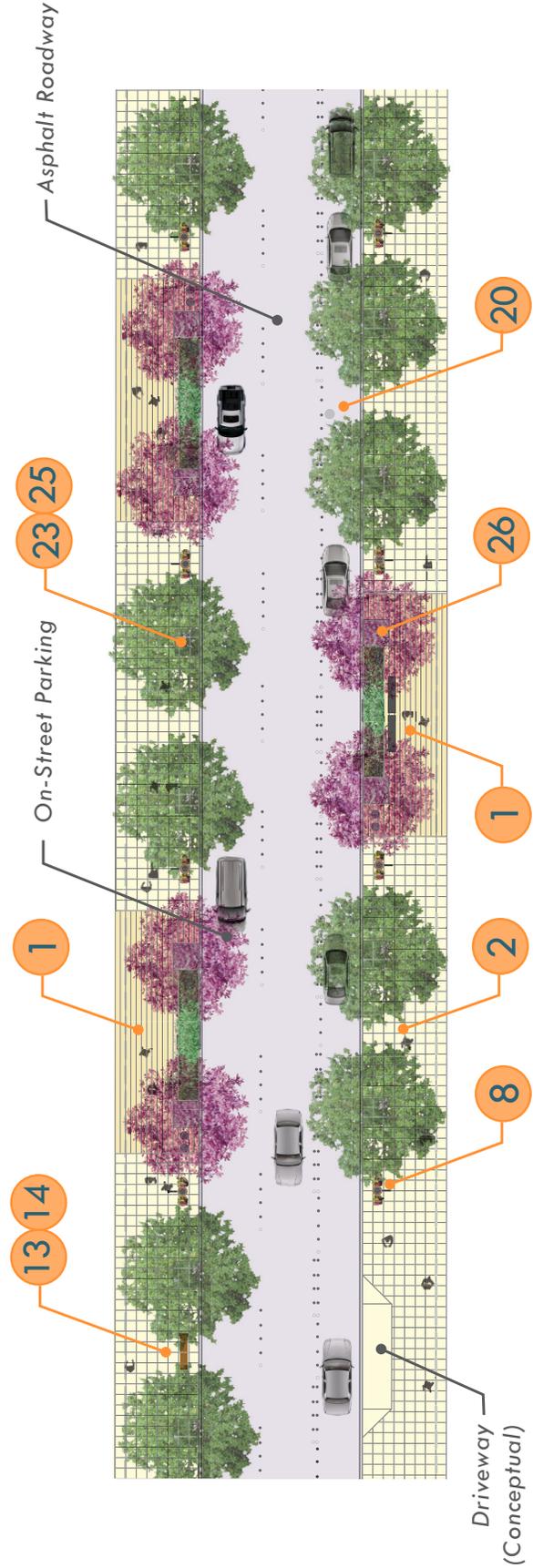
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*See Appendix B for specific roadway sections including number of travel lanes, on-street parking requirements and multi-modal facilities.

STREETSCAPE TYPE 3B REQUIRED ELEMENTS

- 1 Pedestrian Amenity Cluster, typ. 30
- 2 Standard Sidewalk Pavement 32
- 8 Pedestrian Lighting*, typ. [60' O.C., paired]..... 38-39
- 13 Standard Bench [three (3) min. per block w/in 100' of intersection] ... 46
- 14 Custom Bench (optional alternate to standard) 48
- 20 Decorative Utility Covers (as required)..... 54
- 23 Standard Street Tree Grate, typ. [26' O.C., paired] 56
- 25 Street Tree, typ. [26' O.C., paired] 58
- 26 Accent Tree, typ. [20' O.C., paired] 59
- 27 Planter (optional)..... 60

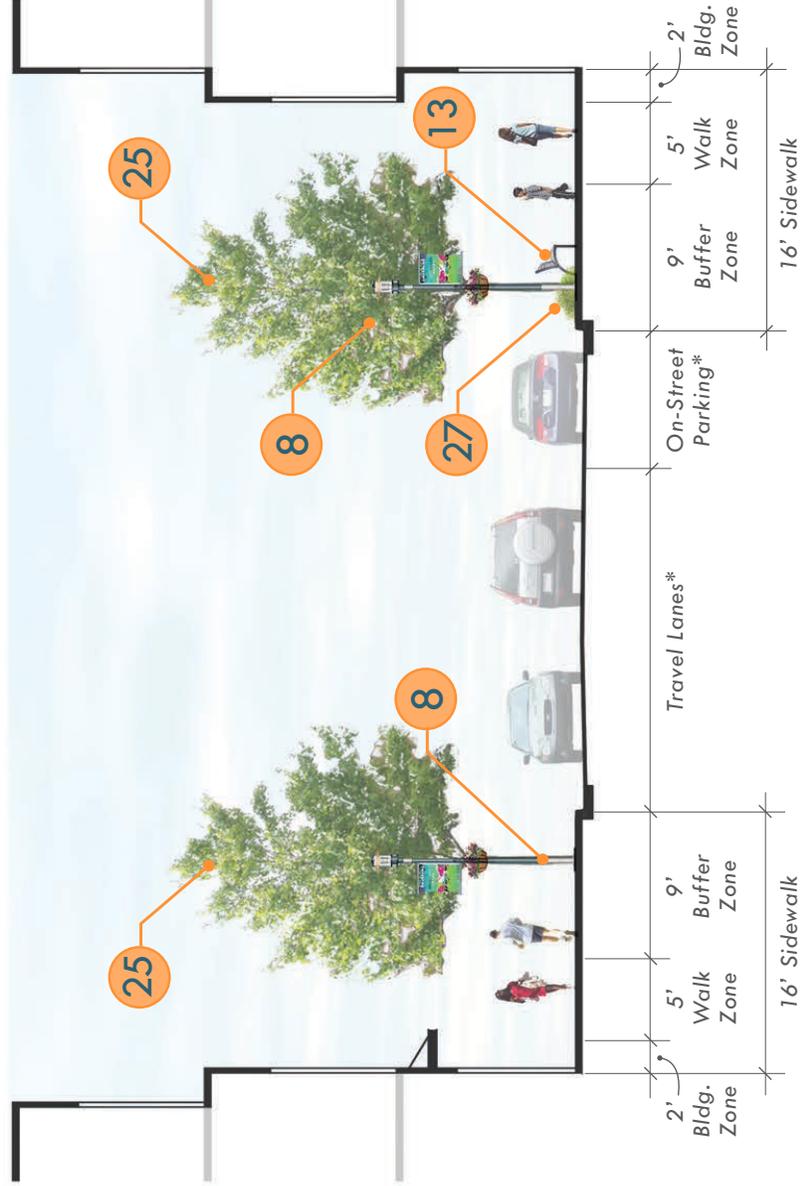


NOTE: Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood

CITY CENTER STREETScape TYPES | STREETScape TYPES 3A & 3B CORE

40TH AVENUE W • 42ND AVENUE W | FROM 196TH STREET SW TO ALDERWOOD MALL BLVD

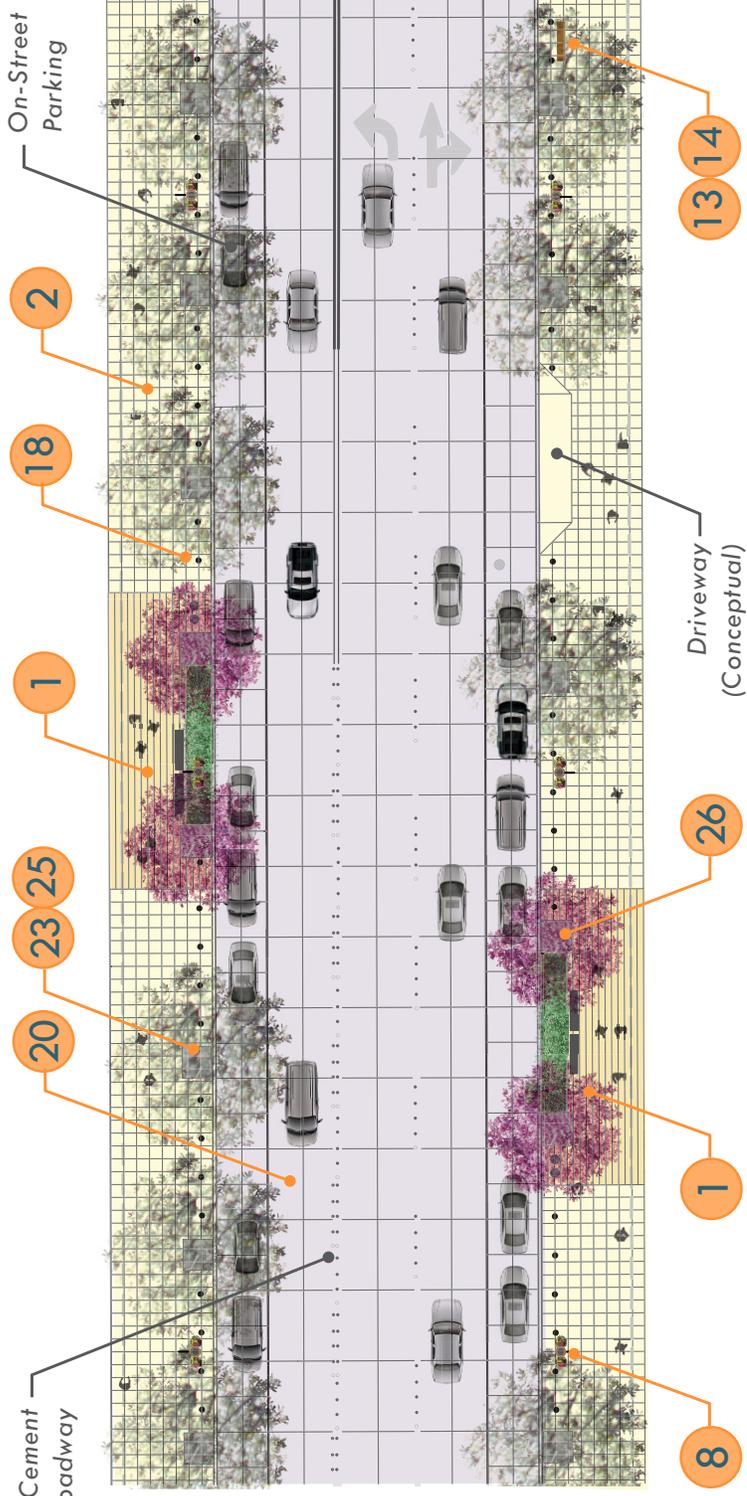
ADD KEY



*See Appendix B for specific roadway sections including number of travel lanes, on-street parking requirements and multi-modal facilities.

STREETSCAPE TYPE 4 REQUIRED ELEMENTS

- 1 Pedestrian Amenity Cluster, typ. 30
- 20 Decorative Utility Covers (as required)..... 54
- 2 Standard Sidewalk Pavement 32
- 23 Standard Street Tree Grate, typ. [26’ O.C., paired] 56
- 8 Pedestrian Lighting, typ. [60’ O.C., paired]..... 40
- 25 Street Tree, typ. [26’ O.C., paired] 58
- 13 Standard Bench [four (4) min. per block w/in 100’ of intersection]..... 46
- 26 Accent Tree, typ. [20’ O.C., paired] 59
- 14 Custom Bench (optional alternate to standard) 48
- 27 Planter (optional)..... 60
- 18 Decorative Bollard, typ. [5’ O.C., 3’ from roadway edge] 52

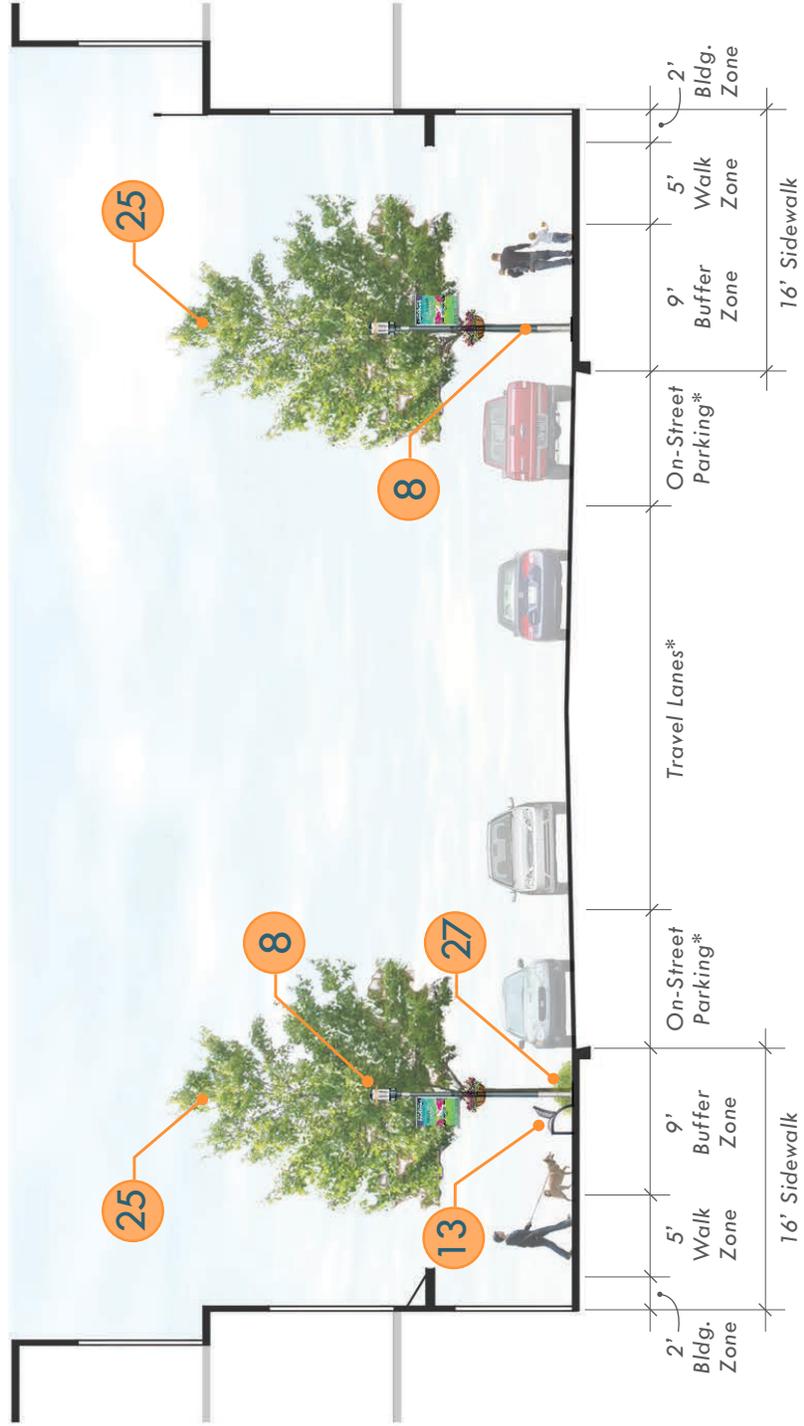


NOTE: Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood

CITY CENTER STREETScape TYPES | STREETScape TYPE 4 "MAIN STREET"

198TH STREET SW

ADD KEY

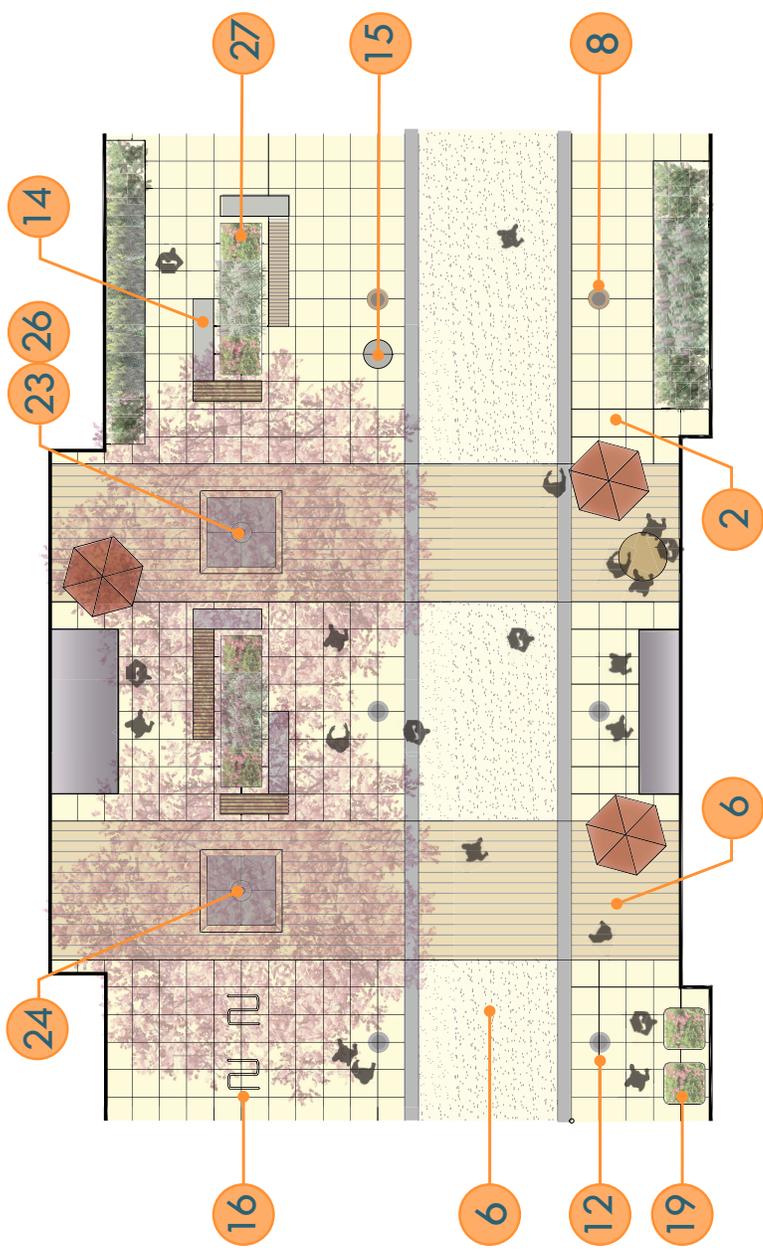


*See Appendix B for specific roadway sections including number of travel lanes, on-street parking requirements and multi-modal facilities.

CITY CENTER STREETSCAPE TYPES | PEDESTRIAN PROMENADE

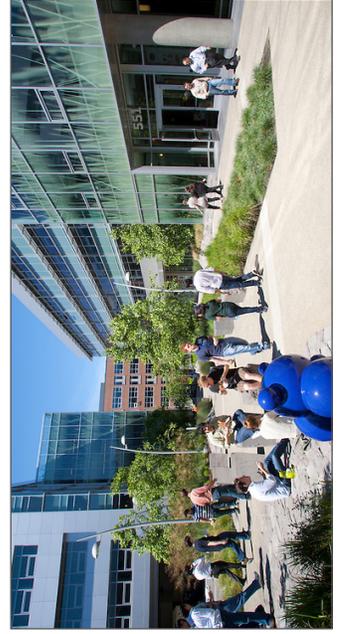
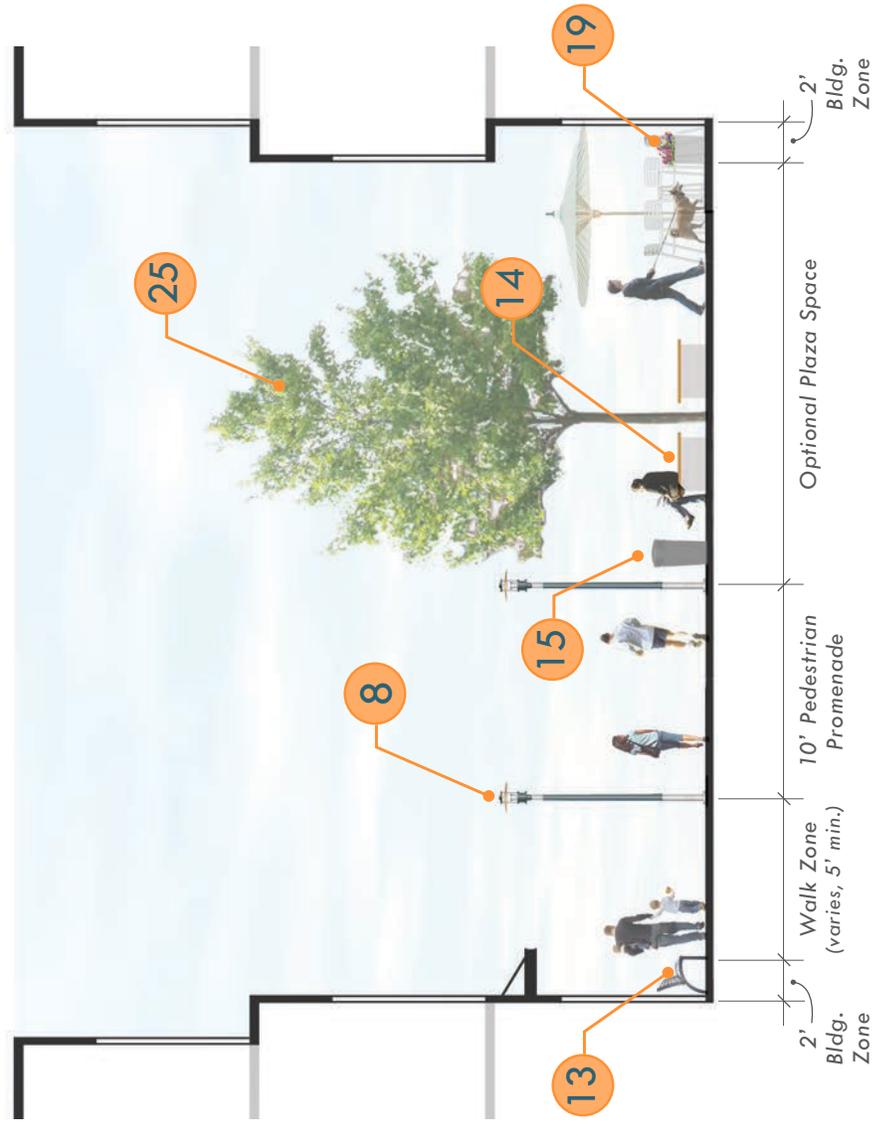
PEDESTRIAN PROMENADE REQUIRED ELEMENTS

- 2 Standard Sidewalk Pavement 32
- 6 Sidewalk Accent Pavement 34
- 8 Pedestrian Lighting, typ. [60' O.C., paired]..... 40
- 12 Accent Lighting, typ. 44
- 13 Standard Bench, typ. 46
- 14 Custom Bench (optional alternate to standard) 48
- 15 Litter & Recycle Receptacle, typ. 50
- 16 Bicycle Rack, typ. 51
- 19 Cement Concrete Planters, typ. 53
- 23 Standard Street Tree Grate, typ. [26' O.C., paired] 56
- 24 Panel Street Tree Grate (optional alternate to standard) 57
- 25 Street Tree, typ. [26' O.C.] 58
- 26 Accent Tree, typ. [20' O.C., paired] 59
- 27 Planter (optional)..... 60



NOTE: Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood

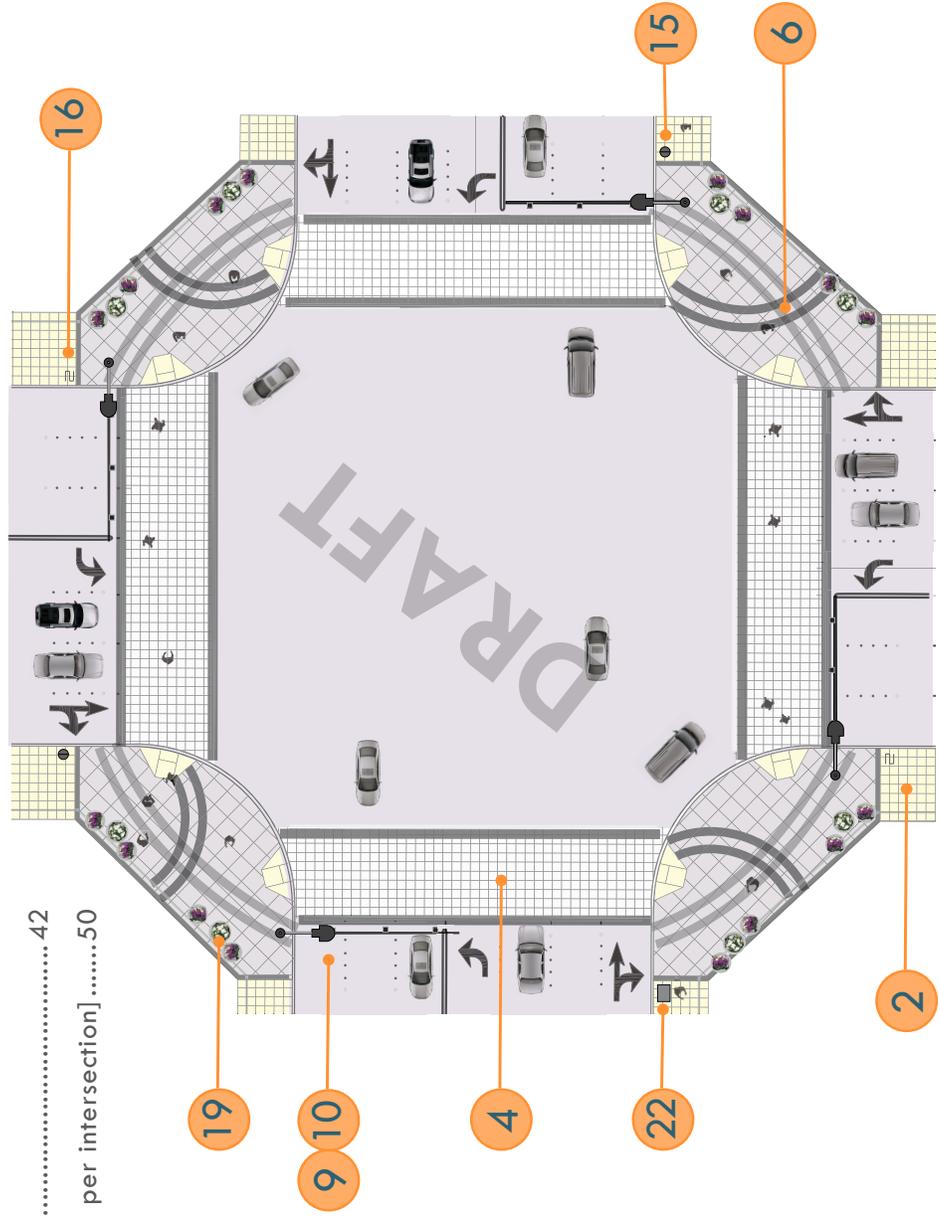
CITY CENTER STREETScape TYPES | PEDESTRIAN PROMENADE



Examples of pedestrian promenade spaces

GATEWAY INTERSECTION REQUIRED ELEMENTS

2	Standard Sidewalk Pavement	32	16	Bicycle Racks, typ. [two (2) per intersection]	51
4	Decorative Crosswalk, typ.	23, 33	19	Cement Concrete Planters, typ. [six (6) at each corner]	53
6	Sidewalk Accent Pavement	23, 34	22	Utility Box	57
9	Signal Pole & Luminaire, typ.	42			
10	Illuminated Street Name Sign, typ.	42			
15	Litter & Recycling Receptacles, typ. [two (2) per intersection]	50			



NOTE: All four (4) corners at the Gateway Intersection shall express a unified, cohesive design. Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood.

CITY CENTER STREETScape TYPES | GATEWAY INTERSECTION

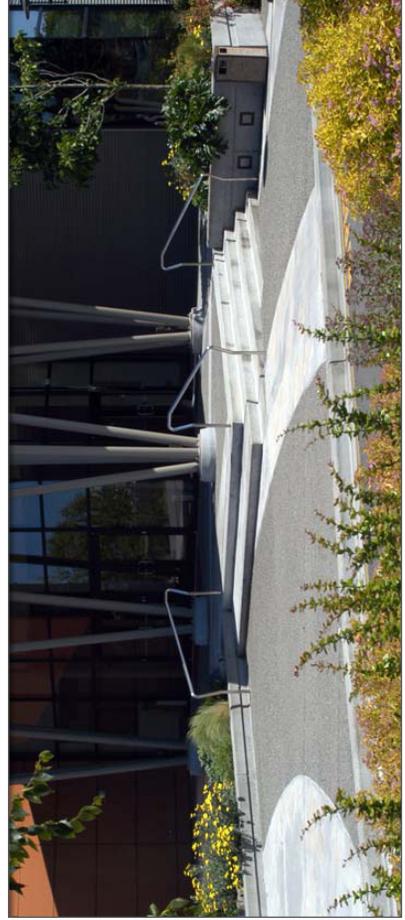
36TH AVENUE W & 196TH STREET SW

GATEWAY INTERSECTION DECORATIVE CROSSWALKS

- **Material:** colored cement concrete pavement
- **Colors:** light gray with dark gray border
- **Pattern:** 1'-0" x 1'-0" grid with border
- **Finish:** sawcut scoring or stamped, medium broom finish; ADA compliant
- **Detectable Tactile Warning Strip:** black
- **Crosswalk Width:** 20'-0"



Example of decorative crosswalk with 1'x1' grid scoring and dark border



Sidewalk accent pavement at Lynnwood Convention Center with a radial pavement motif and smooth accent bands

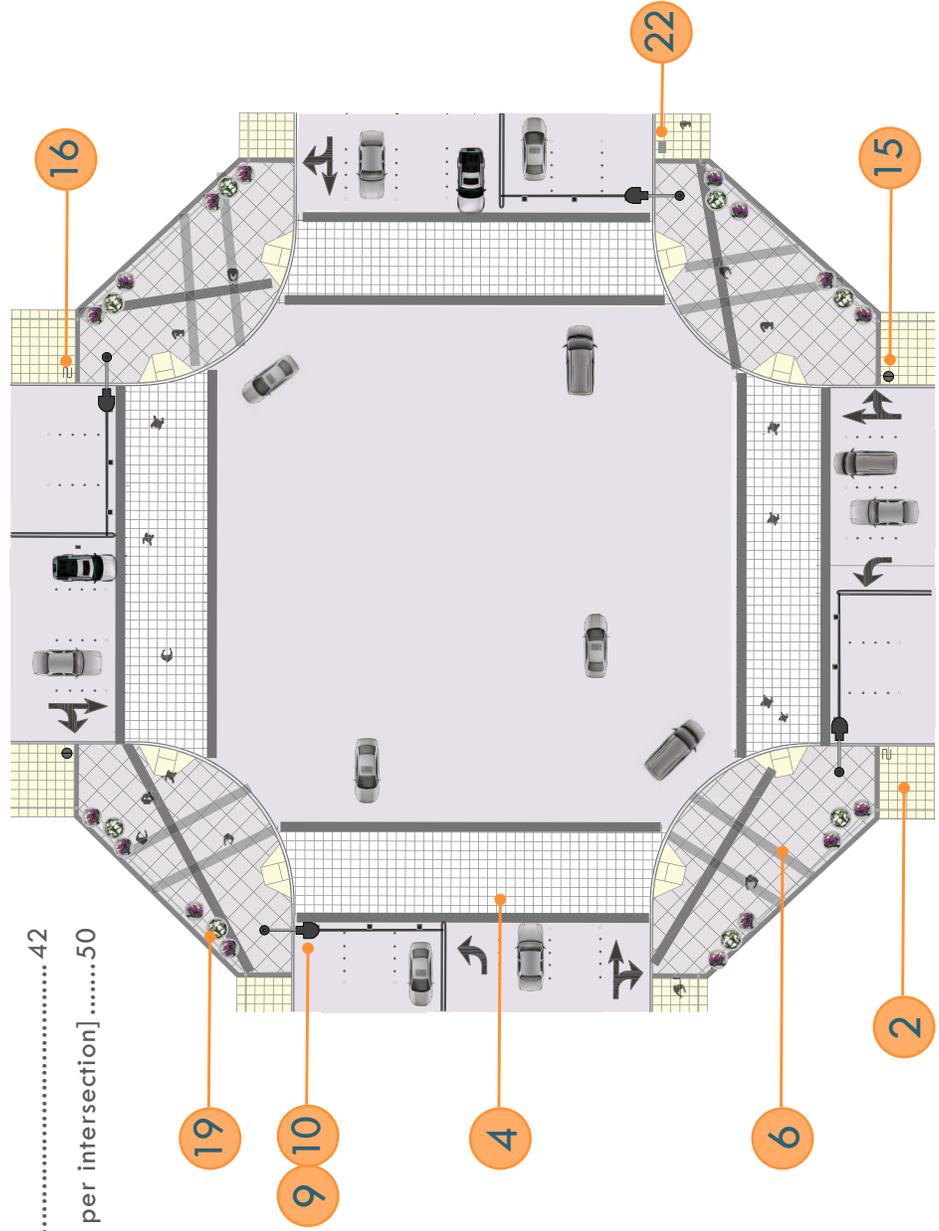
GATEWAY INTERSECTION SIDEWALK ACCENT PAVEMENT

- **Material:** colored cement concrete pavement
- **Colors:** neutral cream with gray border/accent
- **Pattern:** 2'-0" x 2'-0" angled grid; continue radial pavement motif from Convention Center to other sidewalk corners at the intersection
- **Finish:** sawcut scoring, smooth or acid-etched finish



PROMINENT INTERSECTION REQUIRED ELEMENTS

- 2 Standard Sidewalk Pavement 32
- 4 Decorative Crosswalk, typ. 23, 33
- 6 Sidewalk Accent Pavement 23, 34
- 9 Signal Pole & Luminaire, typ. 42
- 10 Illuminated Street Name Sign, typ. 42
- 15 Litter & Recycling Receptacles, typ. [two (2) per intersection] 50
- 16 Bicycle Racks, typ. [two (2) per intersection] 51
- 19 Cement Concrete Planters, typ. [six (6) at each corner] 53
- 22 Utility Box 57



NOTE: All four (4) corners at the Prominent Intersection shall express a unified, cohesive design. Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood.

CITY CENTER STREETScape TYPES | PROMINENT INTERSECTION

44TH AVENUE W & 196TH STREET SW

GATEWAY INTERSECTION DECORATIVE CROSSWALKS

- **Material:** colored cement concrete pavement
- **Colors:** light gray with dark gray border
- **Pattern:** 1'-0" x 1'-0" grid with border
- **Finish:** sawcut scoring or stamped, medium broom finish; ADA compliant
- **Detectable Tactile Warning Strip:** black
- **Crosswalk Width:** 20'-0"



Example of accent pavement with 2'x2' angled grid scoring and linear accent bands



Example of decorative crosswalk with 1'x1' grid scoring and dark border

GATEWAY INTERSECTION SIDEWALK ACCENT PAVEMENT

- **Material:** colored cement concrete pavement
- **Colors:** light gray with dark gray accents
- **Pattern:** 2'-0" x 2'-0" angled grid with linear accent bands
- **Finish:** sawcut scoring or stamped, light broom finish with smooth accent bands



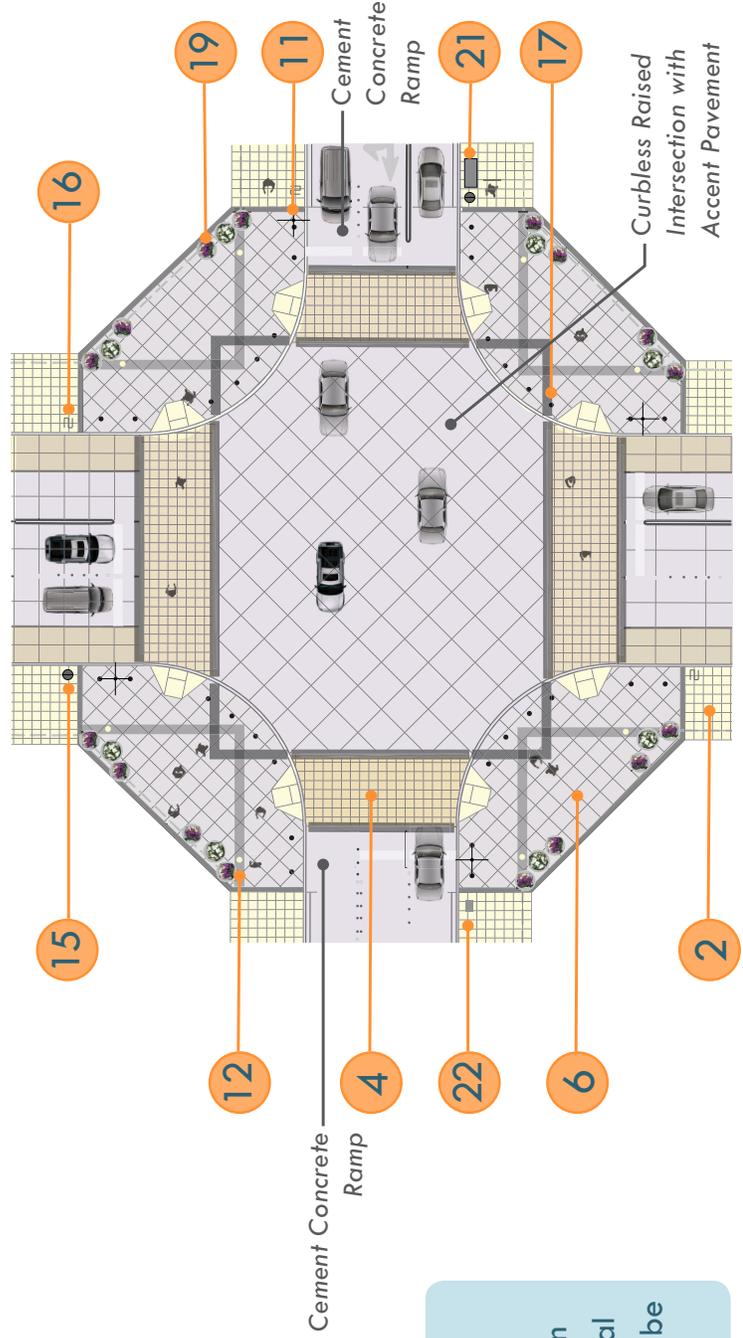
Example of light acid-etched cement concrete pavement with smooth accent bands

CITY CENTER STREETSCAPE TYPES | CELEBRATORY CORNERS

198TH STREET SW & 44TH AVENUE W • 198TH STREET SW & 42ND AVENUE W • 198TH STREET SW & 40TH AVENUE W

CELEBRATORY CORNER REQUIRED ELEMENTS

2	Standard Sidewalk Pavement	..... 32
4	Decorative Crosswalk, typ.	..... 23, 33
6	Sidewalk Accent Pavement	..... 23, 34
11	Street Sign, typ.	..... 43
12	Accent Lighting	..... 45
15	Litter & Recycling Receptacles, typ. [two (2) per intersection]	..... 50
16	Bicycle Racks, typ. [two (2) per intersection]	..... 51
17	Heavy Duty Security Bollard, typ. [seven (7) at each corner, 5' O.C.]	..... 52
18	Decorative Bollard, typ. [5' O.C., 3' from roadway edge]	..... 52
19	Cement Concrete Planters, typ. [six (6) at each corner]	..... 53
21	Media Storage	..... 55
22	Utility Box	..... 57



NOTE: All four (4) corners at the Celebratory Corners shall express a unified, cohesive design. Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood.

CITY CENTER STREETScape TYPES | CELEBRATORY CORNERS

198TH STREET SW & 44TH AVENUE W • 198TH STREET SW & 42ND AVENUE W • 198TH STREET SW & 40TH AVENUE W

GATEWAY INTERSECTION DECORATIVE CROSSWALKS

- **Material:** colored cement concrete pavement
- **Colors:** neutral cream with dark gray border
- **Pattern:** 1'-0" x 1'-0" grid with border
- **Finish:** sawcut scoring or stamped, medium broom finish; ADA compliant
- **Detectable Tactile Warning Strip:** black
- **Crosswalk Width:** 20'-0"

GATEWAY INTERSECTION ROADWAY ACCENT PAVEMENT

- **Material:** colored cement concrete pavement
- **Colors:** neutral cream and shades of gray
- **Pattern:** 5'-0" x 5'-0" grid with interlocking border
- **Finish:** sawcut scoring, light broom finish
- Pavement type conforms to City of Lynnwood roadway standards



Example of decorative crosswalk with 1'x1' grid scoring and dark border

GATEWAY INTERSECTION SIDEWALK ACCENT PAVEMENT

- **Material:** colored cement concrete pavement
- **Colors:** light gray with dark gray accents
- **Pattern:** 2'-0" x 2'-0" angled grid with interlocking rectangular accent bands/border
- **Finish:** sawcut scoring or stamped, light broom finish with smooth accent bands



Example of sidewalk accent pavement with interlocking accent bands

CITY CENTER STREETScape STANDARDS



STREETScape STANDARDS

- Pedestrian Amenity Cluster Standards30
- Hardscape Standards.....32
- Illumination & Signage Standards36
- Street Furniture Standards.....46
- Utility Standards54
- Street Tree & Landscape Standards.....56



PEDESTRIAN AMENITY CLUSTERS

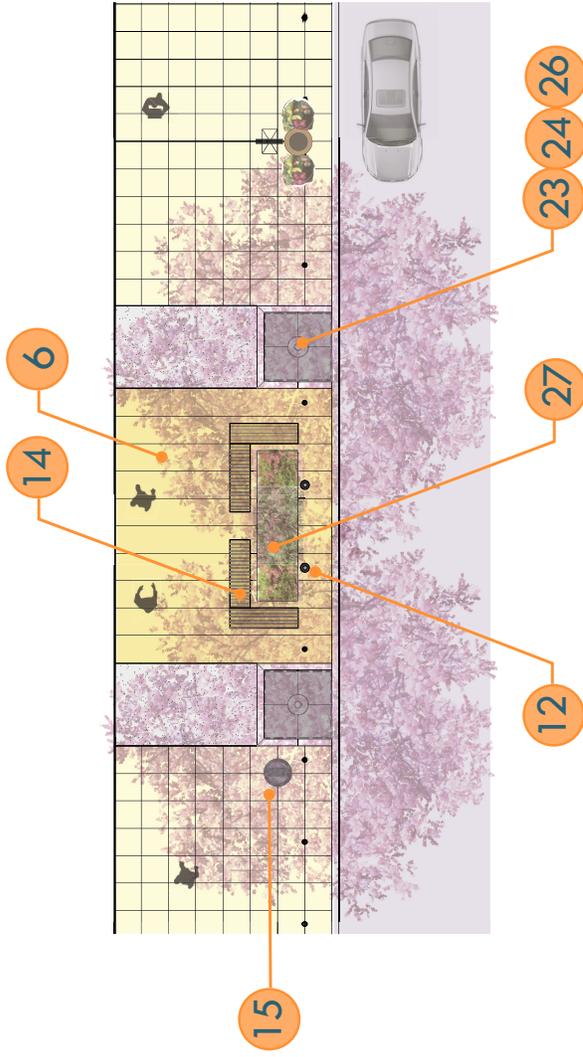
1 STREETScape TYPES 3A & 3B • STREETScape TYPE 4

PEDESTRIAN AMENITY CLUSTER REQUIRED ELEMENTS

- 6 Sidewalk Accent Pavement 23, 34
- 12 Accent Lighting, typ. (optional) 44
- 13 Standard Bench, typ. [two (2) min.] (not shown) 46
- 14 Custom Bench (optional alternate to standard, shown) 48
- 15 Litter & Recycling Receptacles, typ. [one (1) min.] 50
- 16 Bicycle Racks, typ. (optional, not shown) 51
- 19 Cement Concrete Planters, typ. (optional, not shown) 53
- 23 Standard Tree Grate [20' O.C.] 56
- 24 Panel Street Tree Grate (optional alternate to standard) 57
- 26 Accent Tree [20' O.C.] 59
- 27 Planter (optional) 60



Example of pedestrian amenity cluster with custom benches, receptacles, accent lighting, planter, and accent pavement



NOTE: Layout shown is for illustrative purposes only; actual roadway and streetscape design to be approved by the City of Lynnwood

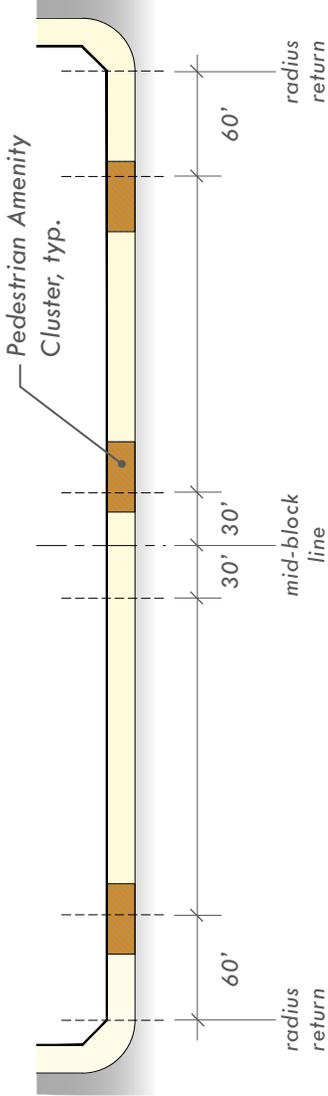
PEDESTRIAN AMENITY CLUSTERS

1

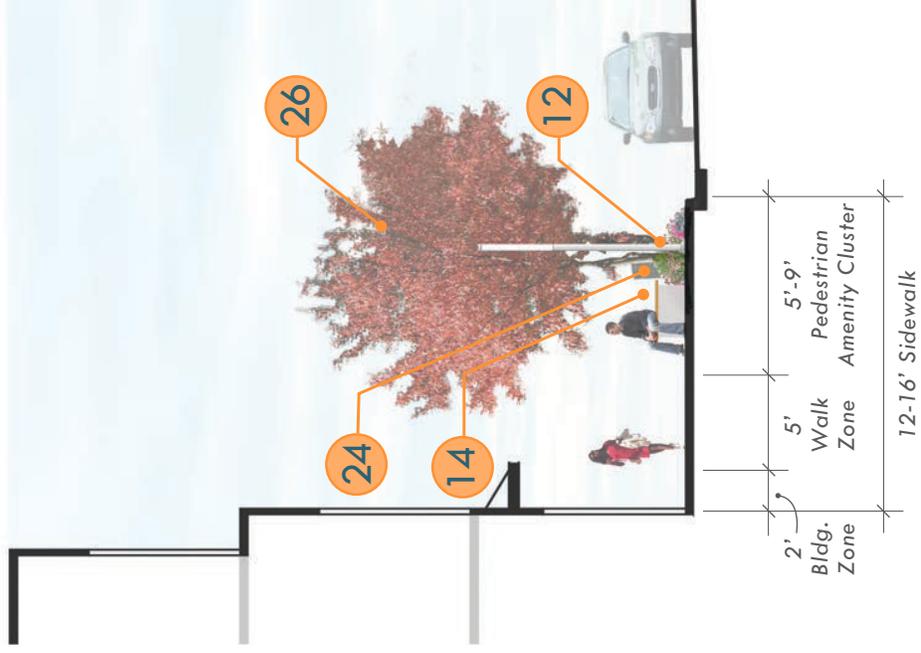
STREETScape TYPES 3A & 3B • STREETScape TYPE 4

PEDESTRIAN AMENITY CLUSTER SPECIFICATIONS

- **Locations:** One (1) cluster within 60'-0" from radius return of each intersection; one (1) cluster min. within 30'-0" of mid-block line (see diagram below)
- **Size:** Approx. 40'-0" X 16'-0"; 5'-0" minimum clear walkway zone



Example of pedestrian amenity cluster with benches, receptacles, bicycle rack, accent lighting, trees in tree grates, and accent pavement



HARDSCAPE STANDARDS | STANDARD SIDEWALK PAVEMENT

2

2 STANDARD SIDEWALK PAVEMENT SPECIFICATIONS

- **Material:** cement concrete pavement
- **Colors:** standard concrete color
- **Pattern:** 2'-0" x 2'-0" grid
- **Finish:** sawcut scoring with light broom finish

STANDARD SIDEWALK LOCATIONS

- Streetscapes Type 1, 2, 3A, 3B and 4 except at Gateway Intersection, Celebratory Corners and Prominent Intersections



Example of standard scored cement concrete pavement

HARDSCAPE STANDARDS | STANDARD CROSSWALK

3

3 STANDARD CROSSWALK SPECIFICATIONS

- **Material:** asphalt with Methyl-Methacrylate (MMA) ladders
- **Crosswalk Width:** 16'-0"
- **Detectable Tactile Warning Strip:** black

STANDARD CROSSWALK LOCATIONS

- Streetscapes Type 1, 2, 3A and 3B except at Gateway Intersection, Celebratory Corners and Prominent Intersections



Example of standard ladder-style crosswalk

HARDScape STANDARDS | DECORATIVE CROSSWALK

GATEWAY INTERSECTION • PROMINENT INTERSECTION • CELEBRATORY CORNERS

4

4 DECORATIVE CROSSWALK CHARACTERISTICS

- **Material:** colored cement concrete pavement
- **Colors:** varies; neutral cream and gray tones
- **Pattern:** 1'-0" x 1'-0" grid with border
- **Finish:** sawcut scoring or stamped, medium broom finish; ADA compliant
- **Detectable Tactile Warning Strip:** black
- **Crosswalk Width:** 20'-0"



Example of decorative crosswalk

HARDScape STANDARDS | PLANTED ROADWAY MEDIANS

5

STREETScape TYPE 1

5 PLANTED ROADWAY MEDIAN CHARACTERISTICS

- **Curb:** cement concrete; 6" minimum height, 12" width typical
- **Planting:** median trees spaced at 30' O.C. typical; shrubs, grasses, groundcovers and perennials (3' maximum height)
- **Median Ends:** 10' min from median radius return
- **Material:** colored or plain cement concrete pavement
- **Pattern:** linear, grid or ashlar; crisp, clean contemporary lines
- **Finish:** sawcut scoring or stamped, no tooled radius on edges, fine texture or light broom finish
- **LID Opportunities:** median trees in Filterra units, modular wetlands, reverse-crowned biofiltration swale



Example of planted median with stamped/scored colored cement concrete

6 SIDEWALK ACCENT PAVEMENT CHARACTERISTICS

- **Description:** accent pavement should express crisp, clean contemporary lines; design to be approved by the City of Lynnwood
- **Material:** colored cement concrete pavement or igneous stone pavers; pavers to be mortar-set with cement concrete rat slab
- **Sizes and shape:** 6" to 30" squares and rectangle
- **Pattern:** linear, grid or ashlar
- **Colors:** neutral cream and all shades of gray
- **Finish:** sawcut scoring or stamped; fine texture, light broom finish or light acid etched



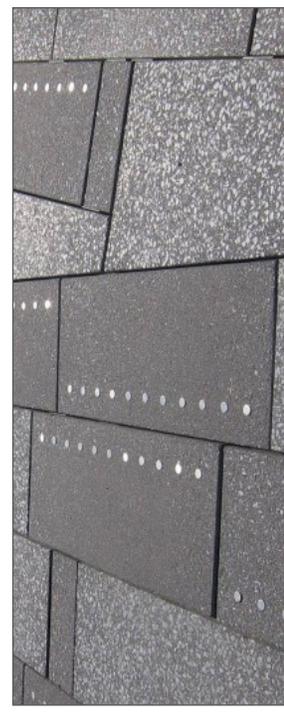
Example of contrasting smooth and stamped cement concrete pavement



Example of stamped colored cement concrete pavement



Example of scored cement concrete pavement with 2'x2' angled grid pattern



Example of mortar-set accent pavers

HARDScape STANDARDS | SIDEWALK ACCENT PAVEMENT

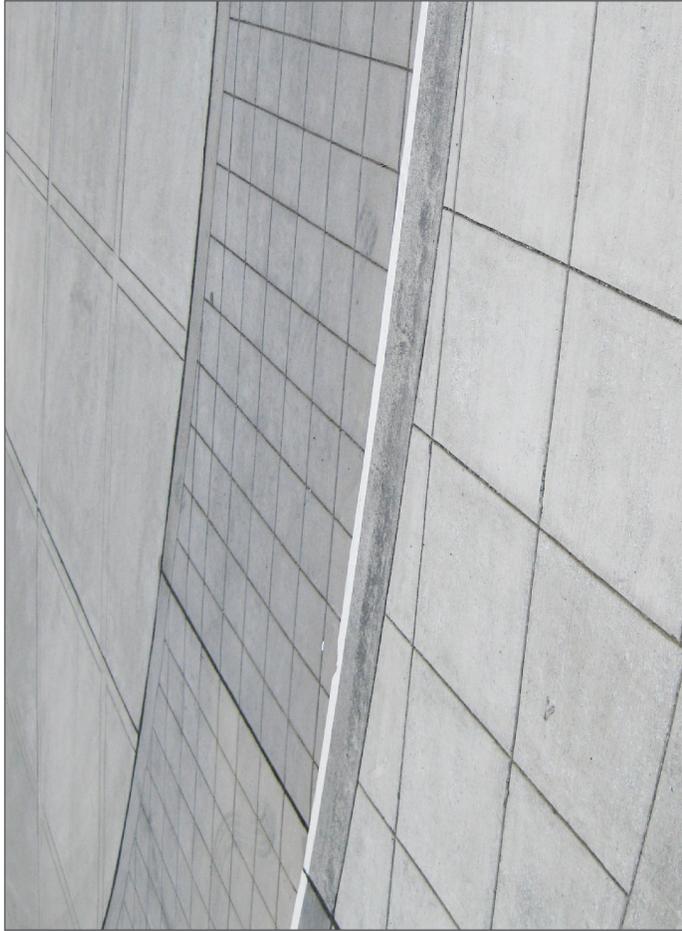
6

SIDEWALK ACCENT PAVEMENT LOCATIONS

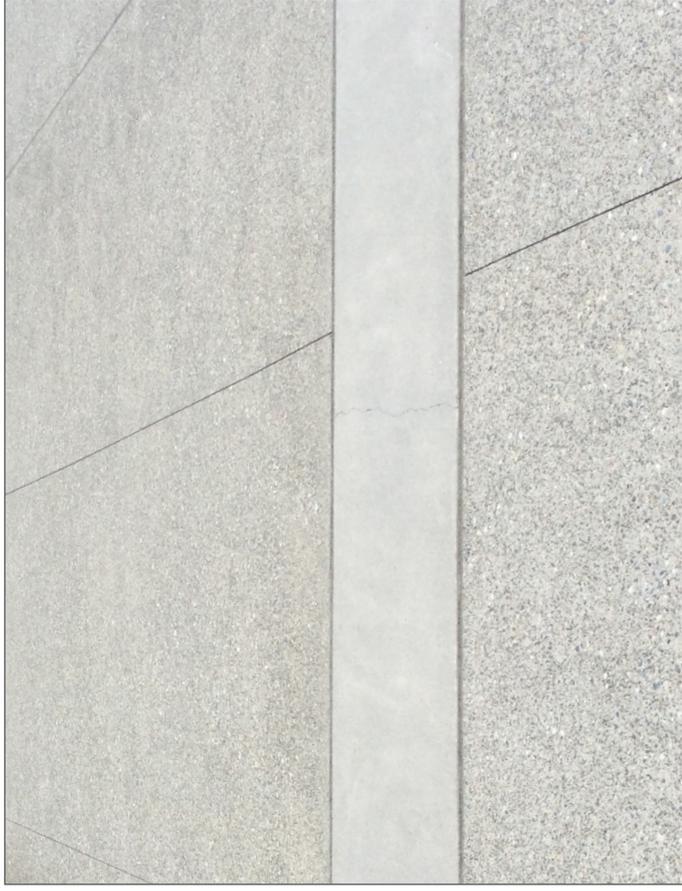
- **Celebratory Corners:** 198th Street SW at 40th Avenue W, 42nd Avenue W and 44th Avenue W (required)
- **Prominent Intersection:** 196th Street SW at 44th Avenue W (required)
- **Gateway Intersection:** 37th Avenue W at 196th Street SW (required)
- **Streetscape Type 4 (optional)**
- **Pedestrian Amenity Clusters (required)**
- **Pedestrian Promenade (required)**



Example of scored colored cement concrete pavement



Example of scored cement concrete pavement with contrasting grid pattern



Example of acid-etched cement concrete pavement with smooth bands

STREETSCAPE TYPE 1

LUMINAIRE SPECIFICATIONS

- **Manufacturer:** Kim Lighting
- **Housing:** aluminum; quick installation system (QIS)
- **Style:** contemporary tapered shape luminaire
- **Lamp:** LED 4000K lamp (neutral white)
- **Lens:** flat lens

7 STREET LIGHTING SPECIFICATIONS

- **Luminaire Model:** Warp9-LED (large)
- **BUG:** B2-U0-G2
- **Mounting Height:** 35'-0"
- **Arm Length & Finish:** 6'-0", clear anodized aluminum
- **Pole:** 8.5" diameter, 0.014"/ft tapered aluminum
- **Pole Color & Finish:** graphite gray; metallic polyester powder coating
- **Base Cover:** Cyclone - BD56
- **Spacing & Pattern:** 120'-0" O.C., staggered
- **Setback:** 2'-6" from face of curb
- **Banner:** 10'-0" length metal, 17'-0" vertical clearance



8 PEDESTRIAN LIGHTING SPECIFICATIONS

- **Luminaire Model:** Warp9-LED (small)
- **BUG:** B1-U0-G2
- **Mounting Height:** 14'-6"
- **Arm Length & Finish:** 2'-0", clear anodized aluminum
- **Pole:** 5.5" diameter, 0.014"/ft tapered aluminum
- **Pole Color & Finish:** graphite gray; metallic polyester powder coating
- **Base Cover:** Cyclone - BD55
- **Spacing & Pattern:** 120'-0" O.C., staggered
- **Setback:** 2'-6" from face of curb

ILLUMINATION & SIGNAGE STANDARDS | STREET & PEDESTRIAN LIGHTING

STREETSCAPE TYPE 2

LUMINAIRE SPECIFICATIONS

- **Manufacturer:** Kim Lighting
- **Housing:** aluminum; quick installation system (QIS)
- **Style:** contemporary tapered shape luminaire
- **Lamp:** LED 4000K lamp (neutral white)
- **Lens:** flat lens

7 STREET LIGHTING SPECIFICATIONS

- **Luminaire Model:** Warp9-LED (large)
- **BUG:** B2-U0-G2
- **Mounting Height:** 25'-0"
- **Arm Length & Finish:** 4'-0", clear anodized aluminum
- **Pole:** 8.5" diameter, 0.014"/ft tapered aluminum
- **Pole Color & Finish:** graphite gray metallic; polyester powder coating
- **Base Cover:** Cyclone - BD56
- **Spacing & Pattern:** 180'-0" O.C., paired
- **Setback:** 2'-6" from face of curb
- **Banner:** 7'-0" length metal, 13'-0" vertical clearance



8 PEDESTRIAN LIGHTING SPECIFICATIONS

- **Luminaire Model:** Warp9-LED (small)
- **BUG:** B1-U0-G2
- **Mounting Height:** 14'-6"
- **Arm length & Finish:** 2'-0", clear anodized aluminum
- **Pole:** 5.5" diameter, 0.014"/ft tapered aluminum
- **Pole Color & Finish:** graphite gray; metallic polyester powder coating
- **Base Cover:** Cyclone - BD55
- **Spacing & Pattern:** 60'-0" O.C., paired
- **Setback:** 3'-0" from face of curb

LUMINAIRE SPECIFICATIONS

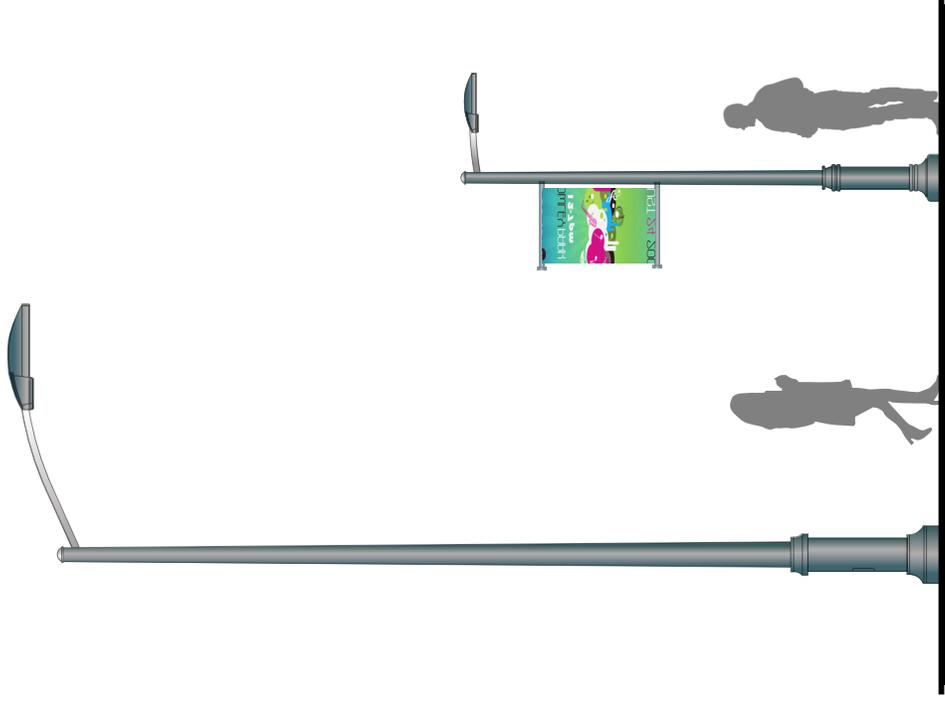
- **Manufacturer:** Kim Lighting
- **Housing:** aluminum; quick installation system (QIS)
- **Style:** contemporary tapered shape luminaire
- **Lamp:** LED 4000K lamp (neutral white)
- **Lens:** flat lens

7 STREET LIGHTING SPECIFICATIONS

- **Luminaire Model:** Warp9-LED (large)
- **BUG:** B2-U0-G2
- **Mounting Height:** 25'-0"
- **Arm Length & Finish:** 4'-0", clear anodized aluminum
- **Pole:** 8.5" diameter, 0.014"/ft tapered aluminum
- **Pole Color & Finish:** graphite gray; metallic polyester powder coating
- **Base Cover:** Cyclone - BD56
- **Spacing & Pattern:** 180'-0" O.C., paired
- **Setback:** 2'-6" from face of curb

8 PEDESTRIAN LIGHTING SPECIFICATIONS

- **Luminaire Model:** Warp9-LED (small)
- **BUG:** B1-U0-G2
- **Mounting Height:** 14'-6"
- **Arm Length & Finish:** 2'-0", clear anodized aluminum
- **Pole:** 5.5" diameter, 0.014"/ft tapered aluminum
- **Pole/Banner Arm Color & Finish:** graphite gray; metallic polyester powder coating
- **Base Cover:** Cyclone - BD55
- **Spacing & Pattern:** 60'-0" O.C., paired
- **Setback:** 2'-6" from face of curb
- **Banner:** dual 4'-0" length cloth, 9'-0" vertical clearance



LUMINAIRE SPECIFICATIONS

- **Manufacturer:** Architectural Area Lighting
- **Housing:** aluminum; quick installation system (QIS)
- **Style:** contemporary tapered shape luminaire
- **Lamp:** LED 4000K lamp (neutral white)

8 PEDESTRIAN LIGHTING SPECIFICATIONS

- **Luminaire Model:** Spectra (large)
- **BUG:** B1-U2-G1
- **Mounting Height:** 15'-0"
- **Luminaire Housing Color:** graphic gray with brushed copper hood
- **Mounting Style:** post top mount
- **Pole:** 5" diameter straight aluminum
- **Base:** 2'-8" tall x 5" diameter straight, clear anodized aluminum
- **Pole/Banner Arm Color & Finish:** graphite gray; metallic polyester powder coating
- **Spacing & Pattern:** 60'-0" O.C., paired
- **Setback:** 2'-6" from face of curb
- **Banner:** dual 4'-0" length cloth, 9'-0" vertical clearance
- **Hanging Flower Baskets:** dual, mounted 9'-6", 7'-0" vertical clearance
- **Irrigation:** drip irrigation for flower baskets



LUMINAIRE SPECIFICATIONS

- **Manufacturer:** Architectural Area Lighting
- **Housing:** aluminum; quick installation system (QIS)
- **Style:** contemporary tapered shape luminaire
- **Lamp:** LED 4000K lamp (neutral white)

8 PEDESTRIAN LIGHTING SPECIFICATIONS

- **Luminaire Model:** Spectra (large)
- **BUG:** B1-U2-G1
- **Mounting Height:** 15'-0"
- **Luminaire Housing Color:** graphic gray with brushed copper hood
- **Mounting Style:** post top mount
- **Pole:** 5" diameter straight aluminum
- **Base:** 2'-8" tall x 5" diameter straight, clear anodized aluminum
- **Pole/Banner Arm Color & Finish:** graphite gray, metallic polyester powder coating
- **Spacing & Pattern:** 60'-0" O.C., paired
- **Setback:** 2'-6" from face of curb
- **Banner:** dual 4'-0" length cloth, 9'-0" vertical clearance
- **Hanging Flower Baskets:** dual, mounted 9'-6", 7'-0" vertical clearance
- **Irrigation:** drip irrigation for flower baskets

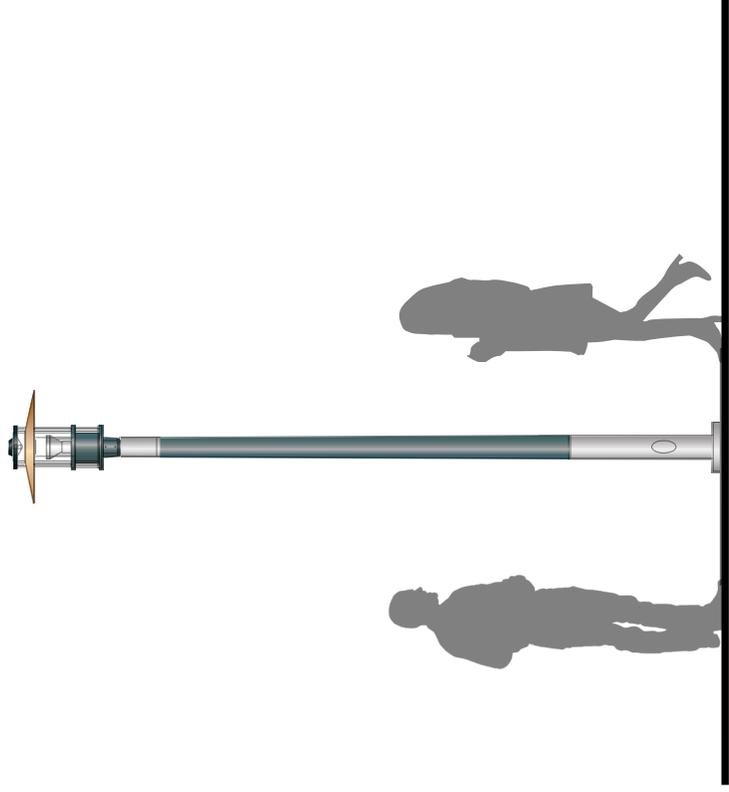


LUMINAIRE SPECIFICATIONS

- **Manufacturer:** Architectural Area Lighting
- **Housing:** aluminum; quick installation system (QIS)
- **Style:** contemporary tapered shape luminaire
- **Lamp:** LED 4000K lamp (neutral white)

8 PEDESTRIAN LIGHTING SPECIFICATIONS

- **Luminaire Model:** Spectra (small)
- **BUG:** B1-U3-G1
- **Mounting Height:** 12'-0"
- **Luminaire Housing Color:** graphic gray with brushed copper hood
- **Mounting Style:** post top mount
- **Pole:** 5" diameter straight aluminum
- **Pole Color & Finish:** graphite gray, metallic polyester powder coating
- **Base:** 2'-8" tall x5" diameter straight aluminum
- **Spacing & Pattern:** 80'-0" O.C. max., paired

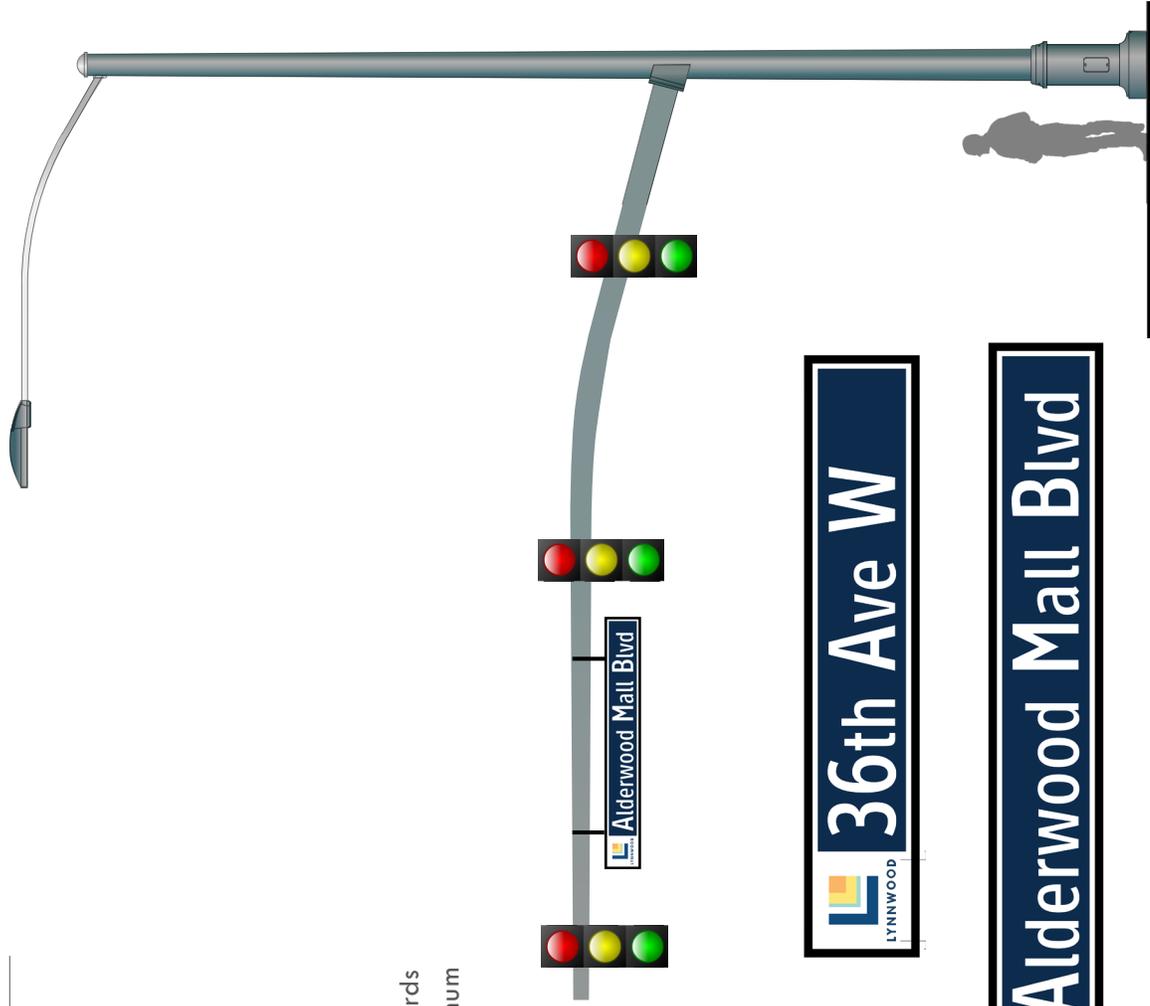


9 SIGNAL POLE AND LUMINAIRE SPECIFICATIONS

- **Luminaire Manufacturer:** Kim Lighting
- **Luminaire Model:** Warp9-LED (large)
- **BUG:** TBD
- **Luminaire Housing:** aluminum; quick installation system (QIS)
- **Luminaire Style:** round shape luminaire
- **Lamp:** LED 4000K lamp (neutral white)
- **Luminaire Mounting Height:** 35'-0"
- **Luminaire Arm Length & Finish:** 6'-7", clear anodized aluminum
- **Luminaire Lens & Shield:** flat lens; house side shield
- **Pole/Signal Arm:** to be determined per City of Lynnwood standards
- **Pole/Signal Arm Color & Finish:** graphite gray and brush aluminum base; metallic polyester powder coating
- **Base Cover:** spun aluminum

10 ILLUMINATED STREET SIGN CHARACTERISTICS

- **Enclosure:** clean profile, LED illumination, black
- **Sign Colors:** dark navy blue and white
- **Sign Material:** aluminum 6061-T6
- **Text Type:** clearview highway type c
- **Text Color:** white
- **Text Height:** 12"; meets FWHA standards, complies to MUTCD
- **Symbols:** City of Lynnwood logo in standard logo colors



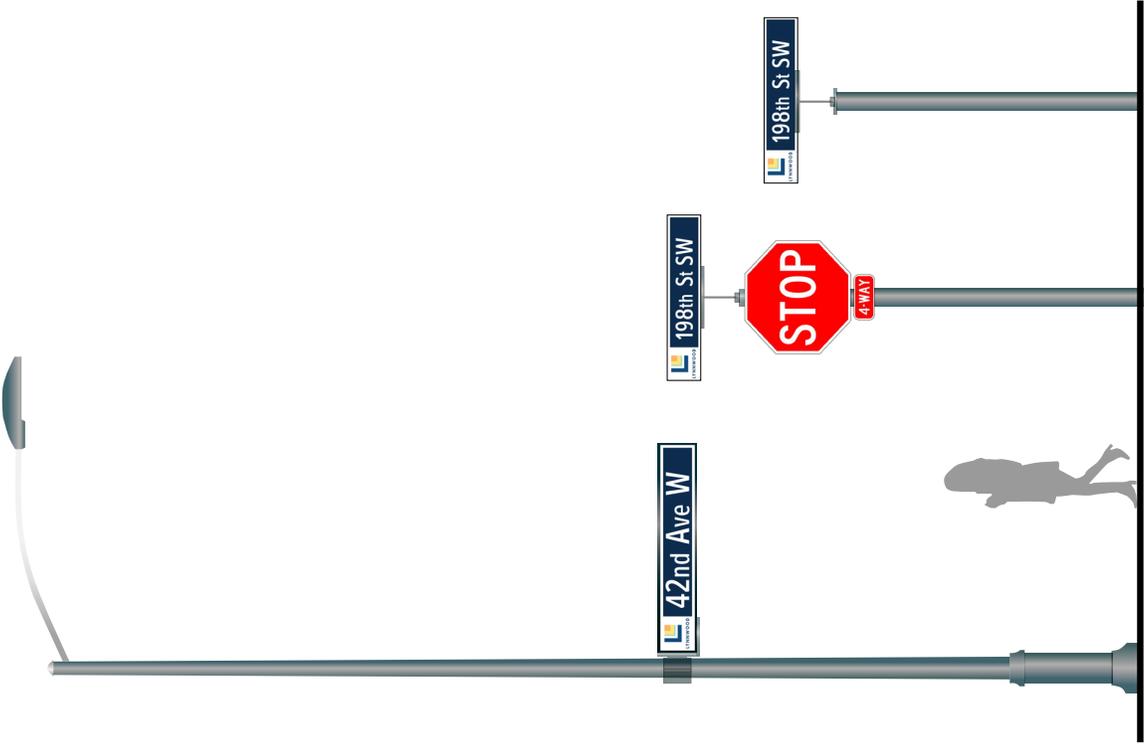
ILLUMINATION & SIGNAGE STANDARDS | STANDARD STREET SIGNS

11

STREETSCAPE TYPES 3A & 3B • STREETScape TYPE 4

11 STANDARD STREET SIGN CHARACTERISTICS

- **Sign Colors:** dark navy blue and white
- **Text Type:** clearview highway type c
- **Text Color:** white
- **Text Height:** 8", meets FWHA standards, complies to MUTCD
- **Symbols:** City of Lynnwood logo in standard logo colors
- **Pole:** 4" diameter straight aluminum
- **Pole Color & Finish:** graphite grey and brush aluminum base; metallic polyester powder coating
- **Mounting Height:** 7' -6" min. clearance
- **Sign Bracket:** clamp-on L bracket with additional bracket around sign blade



12 VERTICAL ACCENT LIGHTING CHARACTERISTICS

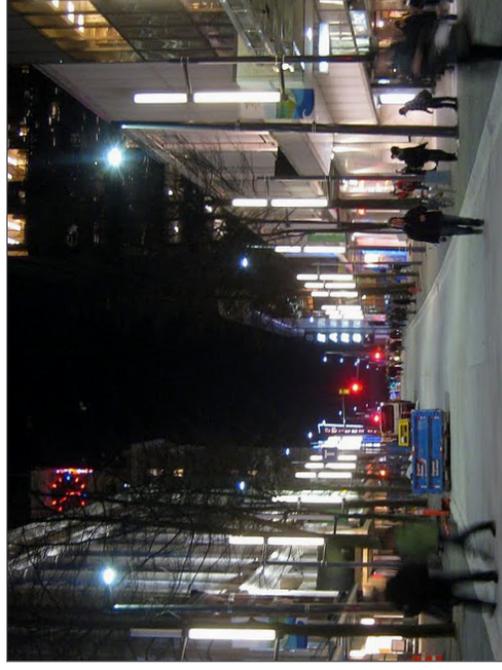
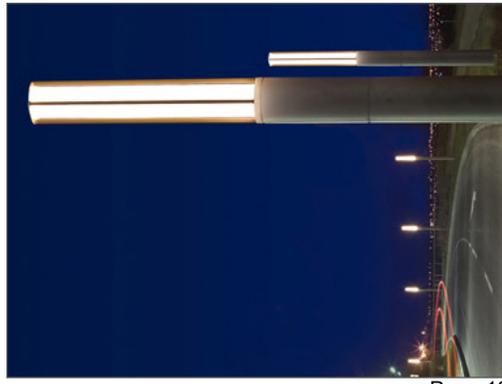
- **Description:** vertical accent lighting should express crisp, clean contemporary lines; lighting to be approved by the City of Lynnwood
- **Material:** white/colored LED, aluminum, stainless steel, frosted outdoor grade acrylic/glass housing
- **Size:** 12'-0" tall
- **Shapes:** cylindrical, squared

VERTICAL ACCENT LIGHTING LOCATIONS

- Pedestrian Amenity Clusters (optional)
- Pedestrian Promenade (required)



Example of vertical accent lighting along a shared-use trail



Examples of vertical accent lighting that expresses crisp, clean, contemporary lines and complements the urban aesthetic

ILLUMINATION & SIGNAGE STANDARDS | ACCENT LIGHTING

12

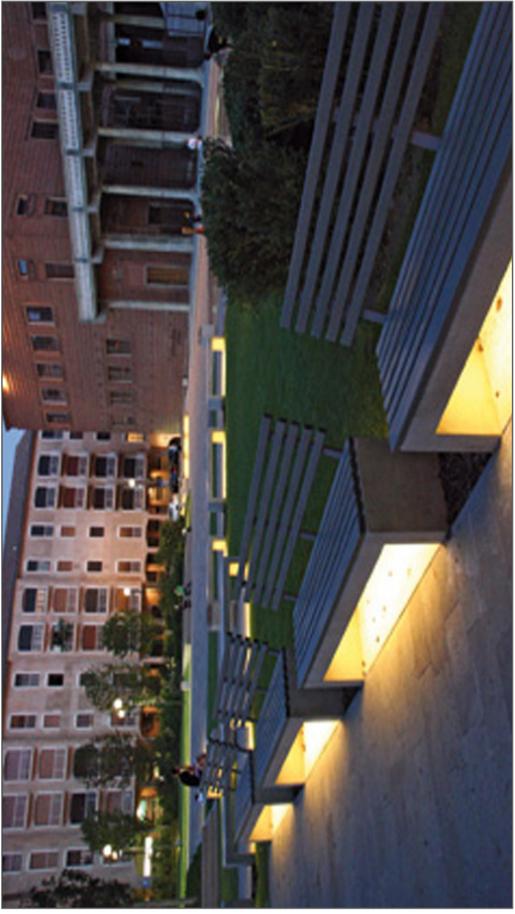
CELEBRATORY CORNERS • PEDESTRIAN AMENITY CLUSTERS • PEDESTRIAN PROMENADE

12 HORIZONTAL ACCENT LIGHTING CHARACTERISTICS

- **Description:** horizontal accent lighting should express crisp, clean contemporary lines; lighting to be approved by the City of Lynnwood
- **Material:** white/colored LED, aluminum, stainless steel, frosted outdoor grade acrylic/glass housing
- **Size:** varies
- **Shapes:** round, square, rectangular

HORIZONTAL ACCENT LIGHTING LOCATIONS

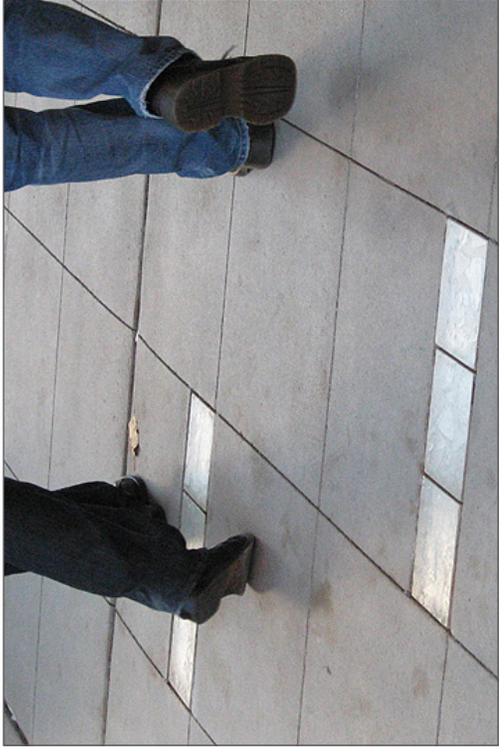
- Celebratory Corners (required)
- Pedestrian Amenity Clusters (optional)
- Pedestrian Promenade (required)



Example of horizontal accent lighting intergrated into custom benches



Examples of in-pavement accent lighting that expresses crisp, clean, contemporary lines and helps to define pedestrian space



13 STANDARD BENCH SPECIFICATIONS

- **Manufacturer:** Landscape Forms
- **Model:** Rest
- **Styles:** backed, backless
- **Materials:** extruded and cast aluminum
- **Metal Color & Finishes:** dark gray; clear anodized aluminum, polyester powder coating
- **Mounting:** surface mount
- **Required Features:** skateboard deterrents

STANDARD BENCH LOCATIONS & SPACING

- Streetscape Type 1: two (2) benches min. per block within 100'-0" of radius return
- Streetscape Type 2: two (2) benches min. per block within 100'-0" of radius return
- Streetscape Types 3A & 3B: three (3) benches min. per block; one (1) bench min. per pedestrian amenity cluster



Rest bench (backless, aluminum)



Rest bench (backed, aluminum)

STREET FURNITURE STANDARDS | STANDARD BENCHES

13

STREETScape TYPE 4 • PEDESTRIAN PROMENADE

13 STANDARD BENCH SPECIFICATIONS

- **Manufacturer:** Landscape Forms
- **Models:** Rest, Mayo, MultipliCITY
- **Styles:** backed, backless
- **Materials:** extruded and cast aluminum (Rest), hardwood (Rest, MultipliCITY), precast concrete (Mayo)
- **Metal Color & Finishes:** dark gray (Rest) and light gray (MultipliCITY); clear anodized aluminum, polyester powder coating
- **Wood Type & Finish:** natural ipe; unfinished
- **Concrete Color & Finish:** light gray; acid wash
- **Mounting:** surface mount (Rest, MultipliCITY), free standing (Mayo)
- **Required Features:** skateboard deterrents

STANDARD BENCH LOCATIONS & SPACING

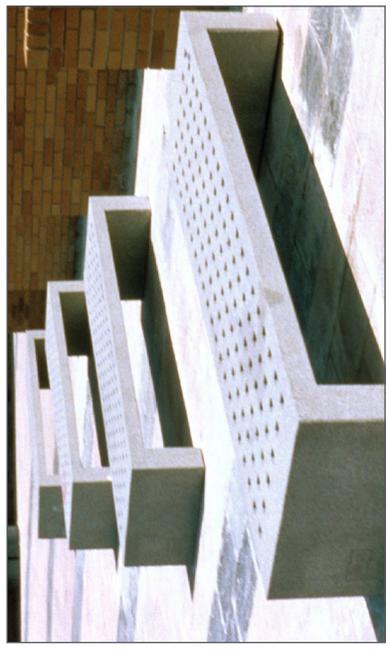
- Streetscape Type 4: four (4) benches min. per block; two (2) benches min. per pedestrian amenity cluster
- Pedestrian Promenade



Multiplicity bench (backed, hardwood & aluminum)



Rest bench (backed, aluminum)



Mayo bench (backless, precast concrete)



Rest bench (backed/backless, hardwood & aluminum)

14 CUSTOM BENCH CHARACTERISTICS

- **Description:** custom benches should express crisp, clean contemporary lines
- **Styles:** backed, backless; stand-alone or integrated with planter edge
- **Materials:** extruded and cast aluminum, stainless steel, hardwood (i.e. ipe), precast concrete
- **Metal Colors & Finishes:** all shades of gray; stainless steel, clear anodized aluminum, brushed aluminum, polyester powder coating
- **Wood Type & Finish:** natural, warm tones; unfinished
- **Concrete Color & Finish:** light gray; acid wash
- **Mounting:** surface mount, embedded, free standing (concrete only)
- **Required Features:** skateboard deterrents



Example of precast concrete custom bench



Example of natural wood bench



Examples of custom stand-alone and built-in benches combining natural wood, concrete and metal that express crisp, clean, contemporary lines

STREET FURNITURE STANDARDS | CUSTOM BENCHES

STREETSCAPE TYPES 3A & 3B • STREETScape TYPE 4 • PEDESTRIAN AMENITY CLUSTERS • PEDESTRIAN PROMENADE

14



Examples of custom stand alone and built-in benches combining natural wood, concrete and metal that express crisp, clean, contemporary lines

CUSTOM BENCHES MAY BE SUBSTITUTED FOR REQUIRED STANDARD BENCHES AT THESE LOCATIONS:

- Streetscape Type 3A & 3B
- Streetscape Type 4
- Pedestrian Amenity Clusters
- Pedestrian Promenade

15 LITTER & RECYCLING RECEPTACLE SPECIFICATIONS

- **Manufacturer:** Forms + Surfaces
- **Model:** Dispatch
- **Style:** split-stream configuration, divided internal baffle plate; vertical articulations; lid cover/top
- **Materials:** cast aluminum body and lid; stainless steel fasteners; rotationally molded, linear, medium density polyethylene bin and base.
- **Sizes:** 36 gal. (43"H x 25.5"W x 21.8"D) or 45 gal. (45.3"H x 26.8"W x 22.9"D)
- **Colors & Finishes:** medium gray (lid & body); clear anodized metal, polyester powder coat (same finish as adjacent standard bench)
- **Mounting:** surface mounted
- **Required Features:** recycle symbol, trash graphic



LITTER & RECYCLING RECEPTACLE LOCATIONS & SPACING

- Streetscape Types 1, 2, 3A, 3B & 4 - two (2) receptacles min. (NW & SE corners) within 30'-0" of radius return at each intersection
- Pedestrian Amenity Clusters (required)
- Pedestrian Promenade (required)



Dispatch litter & recycling receptacle

16 BICYCLE RACK SPECIFICATIONS

- **Manufacturer:** Landscape Forms
- **Model:** Flo multi-rack
- **Style:** multi-rack; u-lock compatible, 2 points of contact
- **Material:** 1.5" O.D. x .120" wall stainless steel tubing, looped
- **Size:** 27.75"D x 25.5"W x 32.5"H
- **Color & Finish:** natural stainless still; bead-blasted
- **Capacity:** three (3) bikes
- **Mounting:** surface mount, embedded

BICYCLE RACK LOCATIONS & SPACING

- Streetscape Types 1, 2, 3A, 3B & 4 - two (2) bicycle racks min. (NE & SW corners) within 30'-0" of radius return at each intersection
- Pedestrian Amenity Clusters (*optional*)
- Pedestrian Promenade (*optional*)



Flo bicycle rack

17 HEAVY DUTY SECURITY BOLLARD SPECIFICATIONS

- **Manufacturer:** Inner City Street Furniture
- **Model:** Antri Ram
- **Style:** cylinder with slanted top
- **Material:** S304 stainless steel
- **Color & Finish:** natural stainless steel; horizontal brushed
- **Sizes:** 3.5", 4", 4.5", 5", 5.5" or 6" diameter x 40" height above grade; 60" overall length, 48" length steel inner core
- **Mounting:** embedded



Anti Ram heavy duty security bollard



HEAVY-DUTY SECURITY BOLLARD LOCATIONS & SPACING

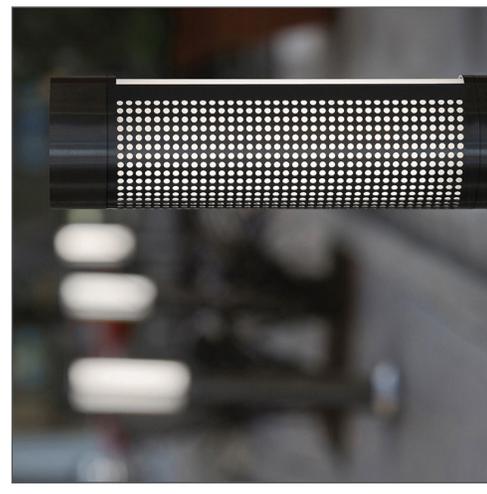
- **Celebratory Corners** - 5'-0" O.C. typ., 3'-0" from edge of roadway

18 DECORATIVE BOLLARD SPECIFICATIONS

- **Manufacturer:** Forms + Surfaces
- **Model:** Light Column Bollard Series 500 or Series 600
- **Style:** cylinder with flat top, lighted
- **Materials:** stainless steel; white/colored LED, frosted outdoor grade acrylic lens, decorative shield
- **Size:** 5" (Series 500) or 6" (Series 600) diameter x 48" height
- **Color & Finishes:** natural stainless steel; satin, polyester powder coated
- **Mounting:** surface mounted or embedded



Light Column decorative bollard with white acrylic shield (L) or optional decorative shield (R)



DECORATIVE BOLLARD LOCATIONS & SPACING

- **Streetscape Type 4** - 5'-0" O.C. typ., 3'-0" from edge of roadway
- **Pedestrian Promenade** - 5'-0" O.C. typ., locate to prevent vehicular access to promenade

STREET FURNITURE STANDARDS | CEMENT CONCRETE PLANTERS

19

19 CEMENT CONCRETE PLANTER SPECIFICATIONS

- **Manufacturers:** Quick Crete, Landscape Forms, Kornegay Design LLC
- **Models:** Cascade, Larkspur, Dune, Square
- **Material:** steel-reinforced concrete
- **Shapes:** tapered round, square or rectangular
- **Sizes:** 2" thick walls; 24", 32", 36" or 48" diameter, 24" to 48" height
- **Colors & Finishes:** sand tan, light brown, gray tones; light sandblast or acid etched
- **Required Features:** drain holes at bottom; sub-irrigation reservoir system; fork truck pockets available

CEMENT CONCRETE PLANTER LOCATIONS

- **Celebratory Corners:** 198th Street SW at 40th Avenue W, 42nd Avenue W and 44th Avenue W
- **Prominent Intersection:** 196th Street SW at 44th Avenue W
- **Pedestrian Amenity Clusters (optional)**
- **Pedestrian Promenade (optional)**



Cascade planter (Quick Crete)



Square planter (Kornegay Design LLC)



Dune planter (Kornegay Design LLC)



Larkspur planter (Landscape Forms)

20 STANDARD MANHOLE COVER SPECIFICATIONS

- **Manufacturer & Model:** Urban Accessories, Inc. Modern
- **Materials:** 100% Recycled Grey Iron, (ASTM A48 class 35b), 100% Recycled Cast Aluminum (ASTM B26)
- **Size:** 24" diameter
- **Finishes:** hot-dipped galvanized steel, natural raw unfinished aluminum



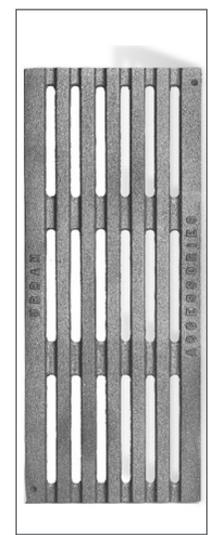
Modern manhole cover

20 CUSTOM MANHOLE COVER CHARACTERISTICS

- **Description:** custom manhole covers should express crisp, clean contemporary lines
- **Material:** 100% Recycled Grey Iron (ASTM A48 class 35b), 100% Recycled Cast Aluminum (ASTM B26)
- **Size:** 24" diameter
- **Finishes:** hot-dipped galvanized steel, natural raw unfinished aluminum
- **Location:** use in place of standard manhole cover at Celebratory Corners

20 TRENCH DRAIN GRATE SPECIFICATIONS

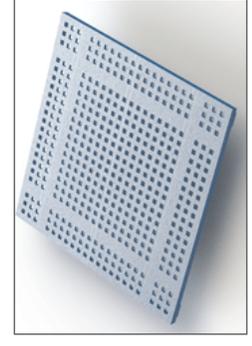
- **Manufacturer & Model:** Urban Accessories, Inc. Flat Rainbow
- **Materials:** 100% Recycled Grey Iron (ASTM A48 class 35b), 100% Recycled Cast Aluminum (ASTM B26)
- **Sizes:** 4"x18", 6"x18", 8"x18", 10"x18"
- **Finishes:** hot-dipped galvanized steel, natural raw unfinished aluminum
- **Required Features:** ADA compliant



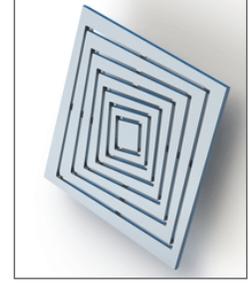
Flat Rainbow trench drain grate

20 DRAIN GRATE SPECIFICATIONS

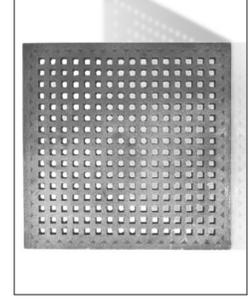
- **Manufacturers & Models:** Ironsmith Market Street or Vertigo; Urban Accessories, Inc. Standard ADA
- **Materials:** 100% Recycled Grey Iron, per ASTM A48 class 35b, 100% Recycled Cast Aluminum, per ASTM B26
- **Sizes:** 8", 12" 16", 18", 14" or 36" square
- **Finishes:** hot-dipped galvanized steel, natural raw unfinished aluminum



Market Street grate



Vertigo grate



Standard ADA grate

UTILITY STANDARDS | MEDIA STORAGE & UTILITY BOXES

21 22

21 MEDIA STORAGE

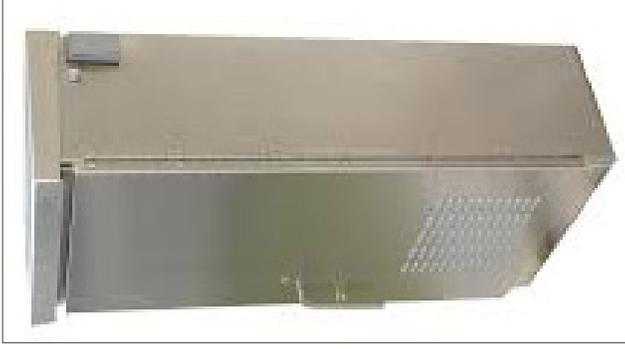
- **Manufacturer:** Lucid Management Group
- **Model:** Multiple Publication Newsrack (MPN)
- **Material:** cast aluminum
- **Size:** 6'-3"W x 1'-6.5"D
- **Color & Finish:** graphite gray; polyester powder coating
- **Location:** Celebratory Corners



MPN media storage

22 UTILITY BOXES

- **Material:** 5052-H32 aluminum, 0.125" thick
- **Finishes:** AA-clear anodized silver (per architectural standard AAMA 611) or brushed aluminum



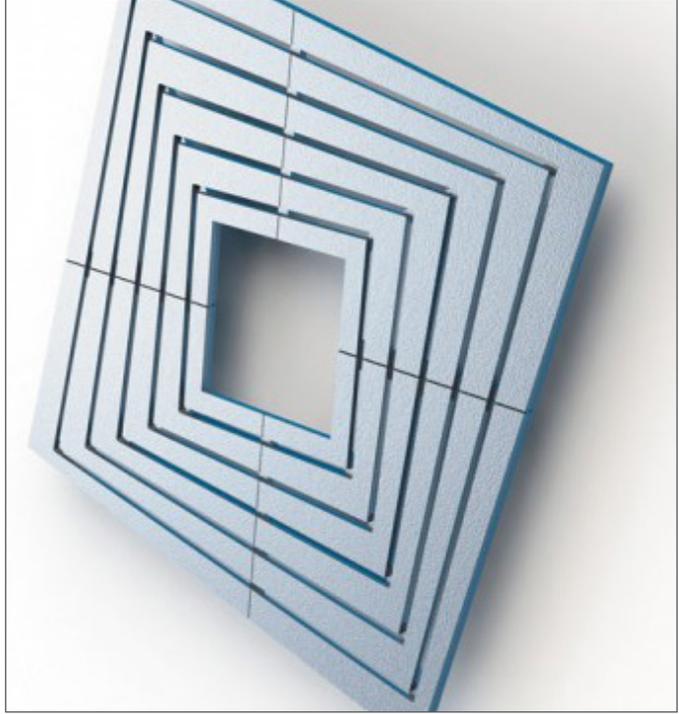
Example of typical utility box

23 STANDARD STREET TREE GRATE SPECIFICATIONS

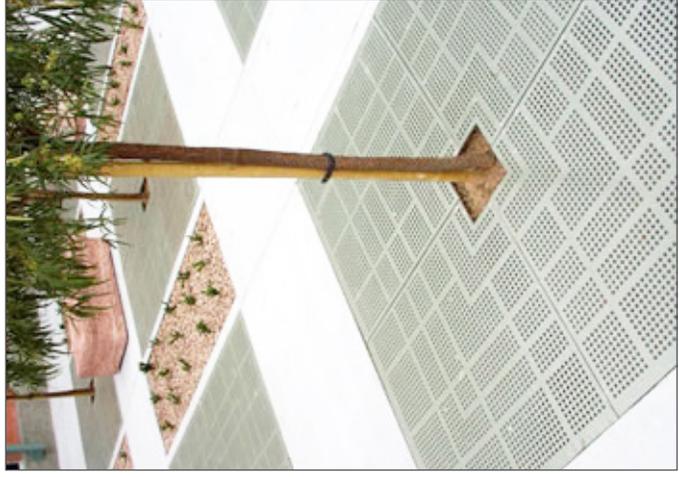
- **Manufacturers:** Iron Age Design, Ironsmith
- **Models:** Divisadero, Vertigo, Market Street
- **Style:** 5'x5' square
- **Materials:** cast aluminum, class 35B gray iron per ASTM A48
- **Colors & Finishes:** all shades of gray; natural raw unfinished aluminum, hot-dipped galvanized iron
- **Frame Angles:** C385 architectural bronze
- **Required Features:** 1/2" max. square opening for A.D.A. compliance; break-away square tree opening; meets coefficient standards for slippage; grate uprights
- **Irrigation:** provide irrigation for all trees in tree grates



Divisadero tree grate (Iron Age Design)



Vertigo tree grate (Ironsmith)



Market Street tree grate (Ironsmith)

STREET TREE & LANDSCAPE STANDARDS | PANEL STREET TREE GRATES

24

STREETSCAPE TYPE 1 • STREETScape TYPE 2 • PEDESTRIAN AMENITY CLUSTERS • PEDESTRIAN PROMENADE

24 PANEL STREET TREE GRATE SPECIFICATIONS

- **Manufacturer:** Jonite
- **Model:** Square Mosaic
- **Style:** 12"x12" modular squares
- **Material:** reconstituted stone, concrete reinforced for added tensile strength
- **Colors & Finish:** all shades of gray; finish per Jonite
- **Standards:** international grating standard BS EN 124.1994; B classification for slip resistance test method AS/NZS 4586
- **Required Features:** 1/2" max. square opening for A.D.A. compliance; break-away square tree opening; meets coefficient standards for slippage; grate uprights
- **Planting:** plant tree opening with shrubs, grasses, groundcovers and perennials
- **Irrigation:** provide irrigation for all trees and plantings in panel tree grates



Square Mosaic modular panel tree grate

PANEL TREE GRATES MAY BE SUBSTITUTED FOR STANDARD TREE GRATES AT THESE LOCATIONS:

- Streetscape Type 1
- Streetscape Type 2
- Pedestrian Amenity Clusters
- Pedestrian Promenade

STREETSCAPE TYPE 2

25 STREET TREE SPECIES

- Acer x freemanii ‘Sienna’/ Sienna Glen Maple
- Pyrus calleryana ‘Aristocrat’/ Aristocrat Pear
- Ginkgo biloba ‘Magyar’/ Magyar Ginkgo (roadway median)

STREET TREE SPACING AND STANDARDS

- 30’ O.C. typical spacing, staggered; two (2) trees spaced between luminaires
- 15’ typical offset from roadway and pedestrian luminaires; 5’ min offset from underground utilities
- Plant trees per diagram, page 59

25 STREET TREE SPECIES

- Acer rubrum ‘Karpick’/ Karpick Maple
- Fraxinus pennsylvanica ‘Rugby’/ Prairie Spire Ash
- Amelanchier laevis ‘JFS-Arb’/ Spring Flurry Serviceberry

STREET TREE SPACING AND STANDARDS

- 30’ O.C. typical spacing, paired; two (2) trees spaced between luminaires
- 15’ typical offset from roadway and pedestrian luminaires; 5’ min. offset from underground utilities
- Plant trees per diagram, page 59

STREETSCAPE TYPE 3A & 3B

25 STREET TREE SPECIES

- Nyssa sylvatica ‘Haymanred’/ Red Rage Tupelo

STREET TREE SPACING AND STANDARDS

- 26’ O.C. typical spacing, paired; two (2) trees spaced between luminaires
- 15’ typical offset from roadway and pedestrian luminaires; 5’ min offset from underground utilities
- Plant trees per diagram, page 59

STREETSCAPE TYPE 4

25 STREET TREE SPECIES

- Acer truncatum x platanoides ‘Keithsform’/ Norwegian Sunset Maple

STREET TREE SPACING AND STANDARDS

- 26’ O.C. typical spacing, paired; two (2) trees spaced between luminaires
- 15’ typical offset from roadway and pedestrian luminaires; 5’ min offset from underground utilities
- Plant trees per diagram, page 59

STREET TREE & LANDSCAPE STANDARDS | ACCENT TREES
PEDESTRIAN AMENITY CLUSTERS • PEDESTRIAN PROMENADE

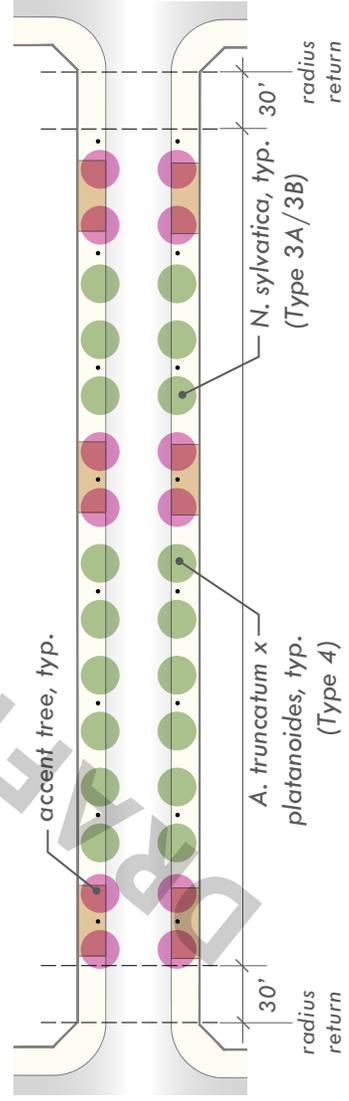
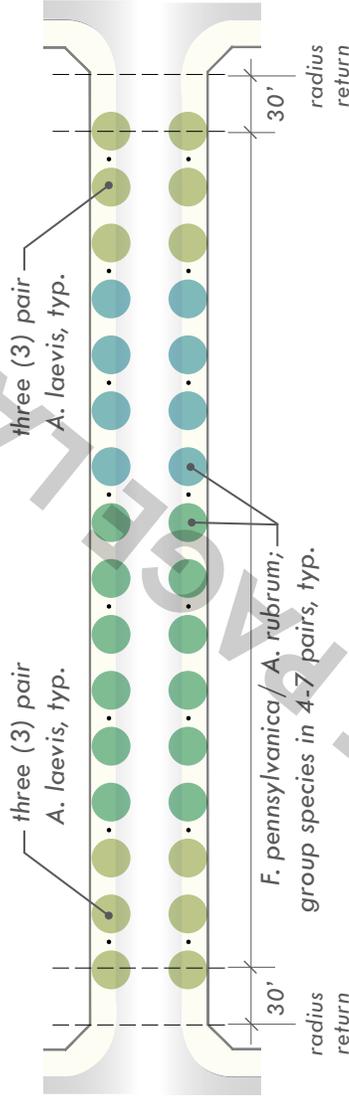
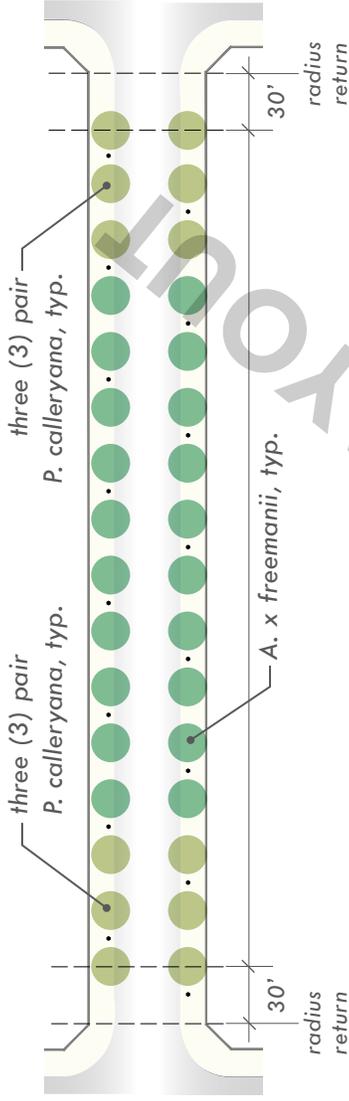
26

26 ACCENT TREE SPECIES

- Prunus 'Berry' / Cascade Snow Cherry
- Malus 'Tschonoskii' / Tschonoskii Crabapple
- Cornus kousa x nuttallii 'KN4-43' / Starlight Dogwood
- Styrax japonicus 'JFS-D' / Snowcone Japanese Snowbell

ACCENT TREE SPACING AND STANDARDS

- Spacing: 20' O.C. min. spacing
- 15' typical offset from roadway and pedestrian lights; 5' min offset from underground utilities
- Plant trees in clusters of a single species at pedestrian amenity cluster locations; no more than 2 accent tree species per block



SHRUBS				HT'	SP'	Suggested Variety
Abelia sp./ Abelia	•		•	3'	4'	'Kaleidoscope'
Berberis thunbergii/ Japanese Barberry	•			2'	2'	'Concorde', 'Goruzam'
Calluna vulgaris/ Scotch Heather	•		•	1.5'	1.5'	'Amethyst', 'Spring Torch'
Cornus stolonifera/ Red-Twig Dogwood (dwarf)		•		2.5'	2.5'	'Kelsey'
Fothergilla gardenii/ Dwarf Fothergilla	•			3'	3'	'Blue Mist'
Hebe sp./ Hebe	•		•	3'	3'	'Western Hills'
Lavandula angustifolia/ English Lavender	•		•	1.5'	1.5'	'Munstead', 'Twinkle Purple'
Mahonia nervosa/ Low Oregon Grape	•	•	•	2'	2'	
Nandina domestica/ Heavenly Bamboo (dwarf)	•		•	2'	2'	'Jaytee', 'Harbor Dwarf'
Polystichum munitum/ Sword Fern	•	•	•	2'	2'	
Potentilla fruticosa/ Shrubby Cinquefoil	•	•		2'	3'	'Sunset'
Spiraea betulifolia/ Birch-Leaf Spirea	•	•	•	3'	3'	'Tor'
Spiraea japonica/ Japanese Spirea	•			2'	3'	'Galen', 'Goldmound'
Vaccinium corymbosum/ Highbush Blueberry			•	2'	2'	'Peach Sorbet'
Weigela florida/ Weigela (dwarf)	•			1.5'	2'	'Elvera'
GRASSES				HT'	SP'	Suggested Variety
Deschampsia cespitosa/ Tufted Hair Grass (dwarf)	•	•	•	1'	2'	'Northern Lights'
Festuca idahoensis/ Idaho Fescue	•	•	•	1.5'	2'	'Siskiyou Blue'
Liriope muscari/ Lilyturf			•	1.5'	2'	'Silvery Sunproof', 'Big Blue'
Miscanthus sinensis/ Maiden Grass (dwarf)	•		•	3'	2'	'Adagio'
Pennisetum alopecuroides/ Fountain Grass (dwarf)	•		•	1'	2'	'Little Bunny', 'Cassian'
Sesleria autumnalis/ Autumn Moor Grass	•		•	1.5'	2'	

- DROUGHT-TOLERANT
- NATIVE/ADAPTED
- EVERGREEN
- PLANT HEIGHT (FEET) *
- PLANT SPREAD (FEET)
- * PLANTS WITHIN THE R.O.W. SHALL BE NO HIGHER THAN 36" MATURE HEIGHT

SHRUBS, GRASSES, GROUNDCOVERS & PERENNIALS SHALL BE INCLUDED AT THE FOLLOWING LOCATIONS:

- Streetscape Type 1 Roadway Medians
- Pedestrian Amenity Cluster Planter Beds
- Panel Tree Grates

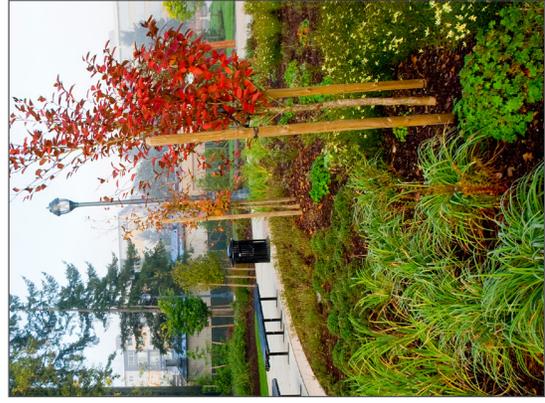
STREET TREE & LANDSCAPE STANDARDS | PLANT LIST

STREETScape TYPE 1 ROADWAY MEDIANS • PEDESTRIAN AMENITY CLUSTER PLANTER BEDS • PANEL TREE GRATES

GROUNDCOVERS				HT*	SP*	Suggested Variety
Arctostaphylos uva-ursi/ Kimmikinnick	•	•	•	.5'	1'	'Vancouver Jade'
Armeria maritima/ Sea Thrift	•	•	•	.5'	.5'	'Lipstick', 'Pink Panda'
Fragaria chiloensis/ Beach Strawberry	•	•	•	.5'	2'	
Geum sp./ Avens	•	•	•	1'	1'	'Mango Lassi'
Helianthemum sp./ Sunrose	•	•	•	1'	1.5'	'Cheviot', 'Fire Dragon', 'Annabel'
Thymus praecox/ Creeping Thyme	•	•	•	.5'	1.5'	'Nutmeg'
PERENNIALS				HT*	SP*	Suggested Variety
Achillea millefolium/ Common Yarrow	•	•	•	2'	2'	'Walther Funke'
Coreopsis verticillata/ Tickseed				1.5'	1.5'	'Moonbeam'
Echinacea purpurea/ Coneflower	•			2'	1.5'	'CBG Cone 2'
Hemerocallis sp./ Daylily	•			1.5'	1.5'	'Swirling Water'
Sedum sp./ Stonecrop	•			1.5'	1.5'	'Matrona'

-  DROUGHT-TOLERANT
-  NATIVE/ADAPTED
-  EVERGREEN
-  PLANT HEIGHT (FEET) *
-  PLANT SPREAD (FEET)

* PLANTS WITHIN THE R.O.W. SHALL BE NO HIGHER THAN 36" MATURE HEIGHT



Examples of shrubs, grasses, groundcovers and perennials within the right-of-way.

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APPENDIX A

City of Lynnwood Standard Details

Street Furniture Details

Bench Type A.....	TBD	Median Details	Median Tree Planting.....	TBD
Bench Type B.....	TBD		Median.....	TBD
Bench Type C.....	TBD	Crosswalk Details		
Heavy Duty Bollard.....	TBD	Standard Crosswalk Striping.....		TBD
Litter & Recycle Receptacle.....	TBD	Gateway Intersection Crosswalk.....		TBD
Bicycle Rack.....	TBD	Prominent Intersection Crosswalk.....		TBD
		Celebratory Corners Intersection.....		TBD

Irrigation Details

Flower Basket Irrigation.....	TBD	Street Sign Details		
Flower Basket Valve Access Box.....	TBD	Street Name Sign Type 1 & 2.....		TBD

Sidewalk Details

Standard Sidewalk Clearances.....	TBD	Street Name Sign Type 3A & 3B.....		TBD
Typical Pedestrian Amenity Layout.....	TBD	Street Name Sign Type 4.....		TBD
Sidewalk Layout.....	TBD			
Standard Sidewalk.....	TBD			
Tree Grate.....	TBD			
Panel Tree Grate.....	TBD			

Illumination Details

Streetscape Type 1 - Street & Pedestrian Luminaire & Pole.....	TBD
Streetscape Type 2 - Street & Pedestrian Luminaire & Pole.....	TBD
Streetscape Type 3A & 3B (1) - Street & Pedestrian Luminaire & Pole.....	TBD
Streetscape Type 3A & 3B (2) - Street & Pedestrian Luminaire & Pole.....	TBD
Streetscape Type 4 - Pedestrian Luminaire & Pole.....	TBD
Promenade - Pedestrian Luminaire & Pole.....	TBD

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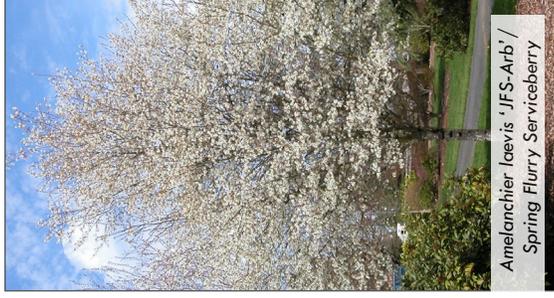
APPENDIX B

City of Lynnwood Roadway Standards

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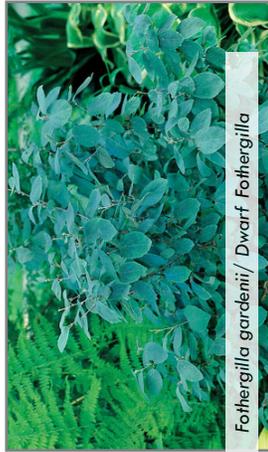
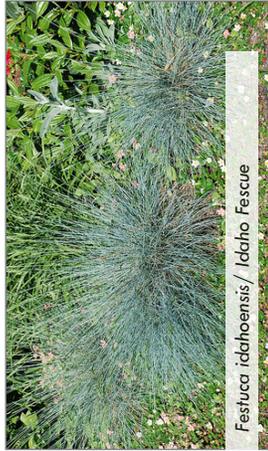
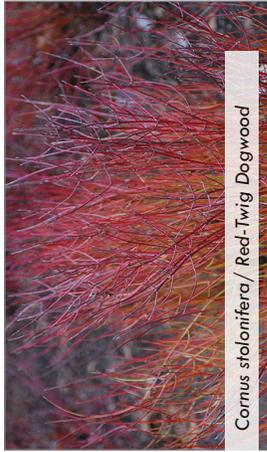
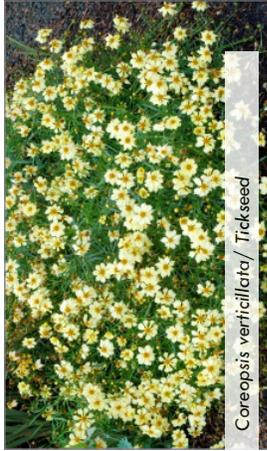
APPENDIX C

Plant Photos: Street, Median and Accent Trees



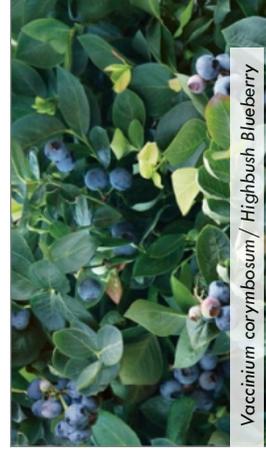
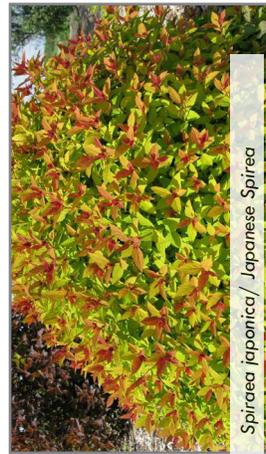
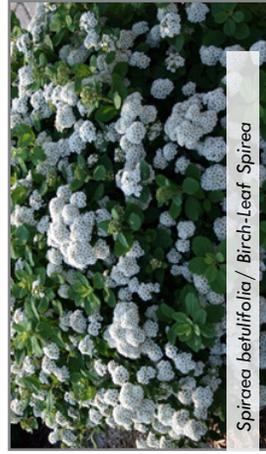
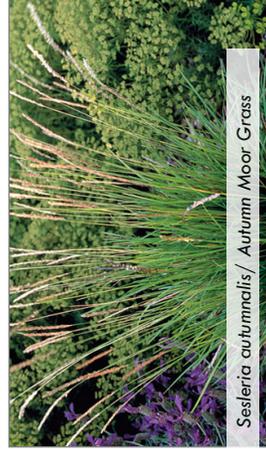
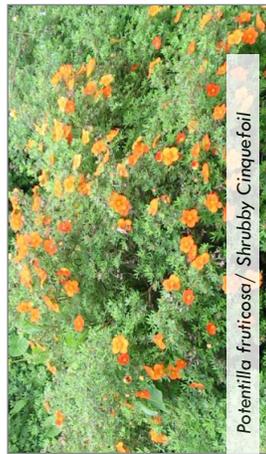
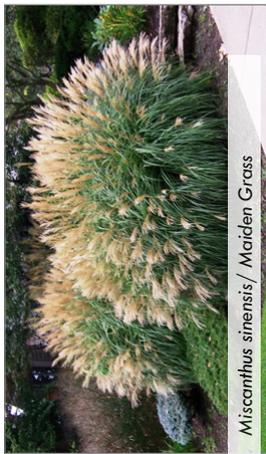
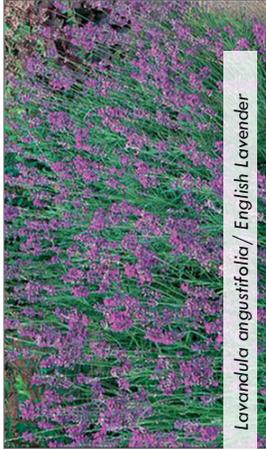
APPENDIX C

Plant Photos: Shrubs, Grasses, Groundcovers & Perennials



APPENDIX C

Plant Photos: Shrubs, Grasses, Groundcovers & Perennials



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APPENDIX D

References & Background Documents

The City Center Streetscape Plan is intended to build upon the following background documents:

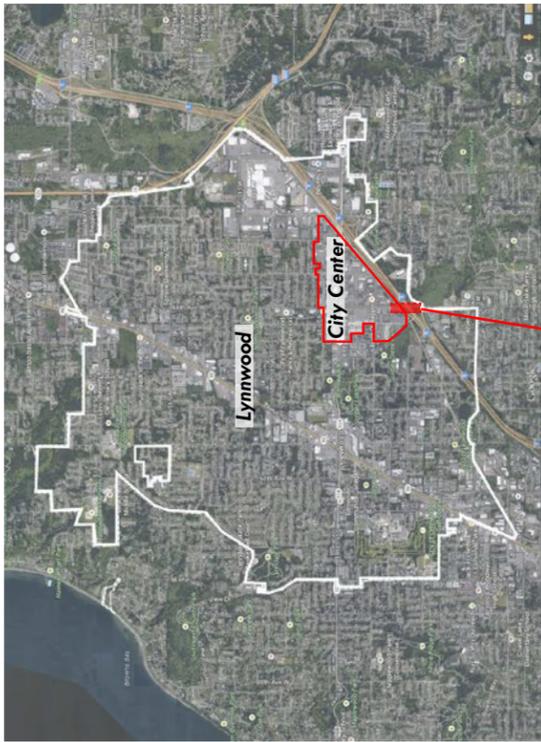
- Draft City Streetscape Plan..... May 2013
- City Center Street Master Plan..... December 2009
- City of Lynnwood’s Community Vision.....January 2009
- City Center Subarea Plan..... September 2007
- City Center Parks Master Plan..... August 2007
- Ordinance 2937 amended City Center zoning LMC 21.60, City Center Grid Streets and City Center Design Guidelines.....March 2012
- Ordinance 2938 City Center Gateways..... March 2012
- Mode Split for City Center Street Master Plan December 2009
- Sound Transit Technical Memorandum - Extension of Light Rail to Lynnwood City Center.....November 2011
- Sound Transit Lynnwood Link DEIS ST Board Preferred Alternative(TBD November 2013)
- Sound Transit Lynnwood Link City of Lynnwood Resolution 2013-10 regarding City’s preferred alignment
- Lynnwood Brand Print Final Report August 2010
- Current items that have been adopted into the City Code or Public Works Standards

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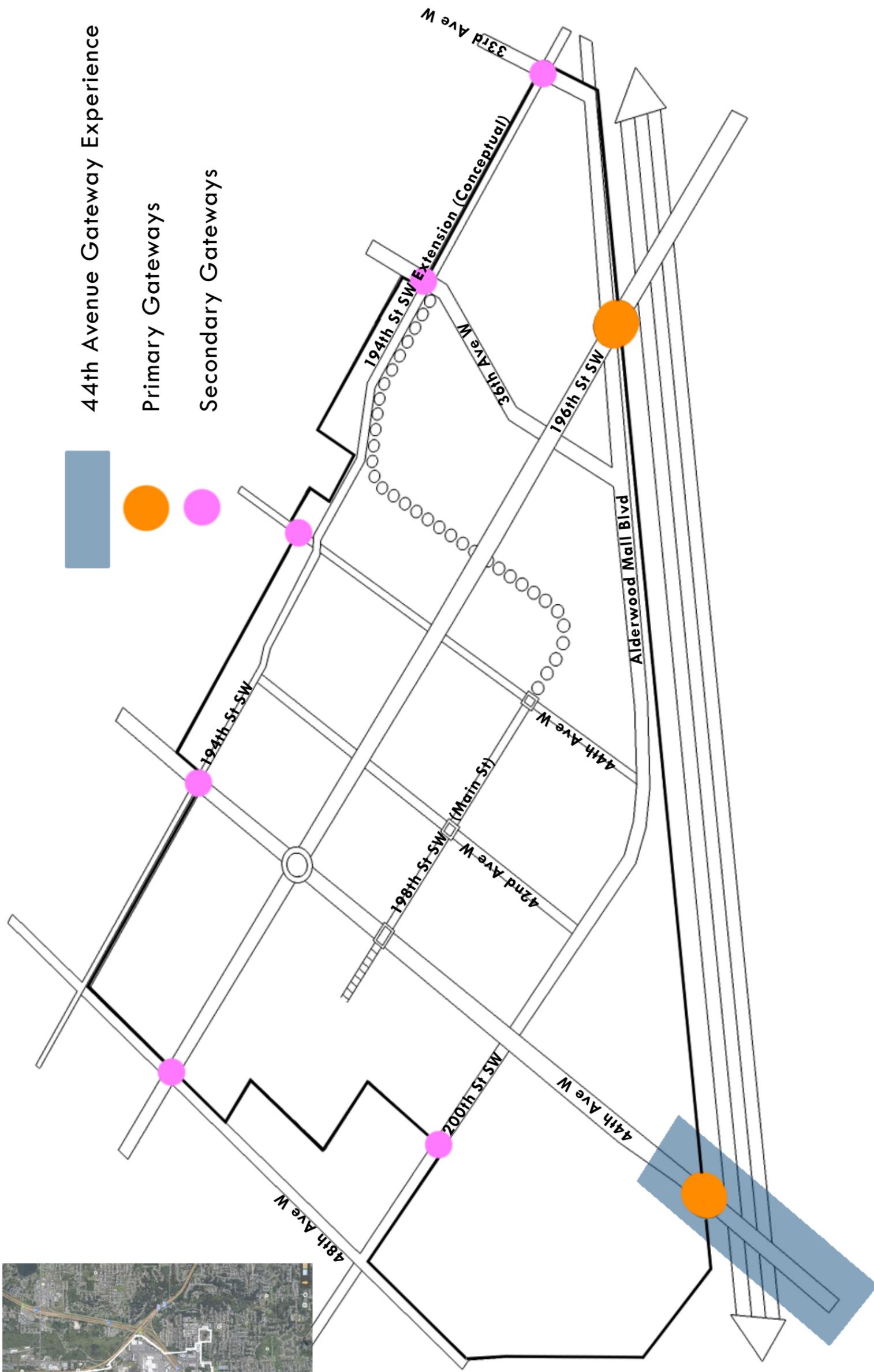


44th Avenue W. Gateway

Lynnwood City Center Gateway on 44th Avenue W



44th Avenue Gateway Experience



44th Avenue W Gateway Experience | Analysis

Gateway Approach



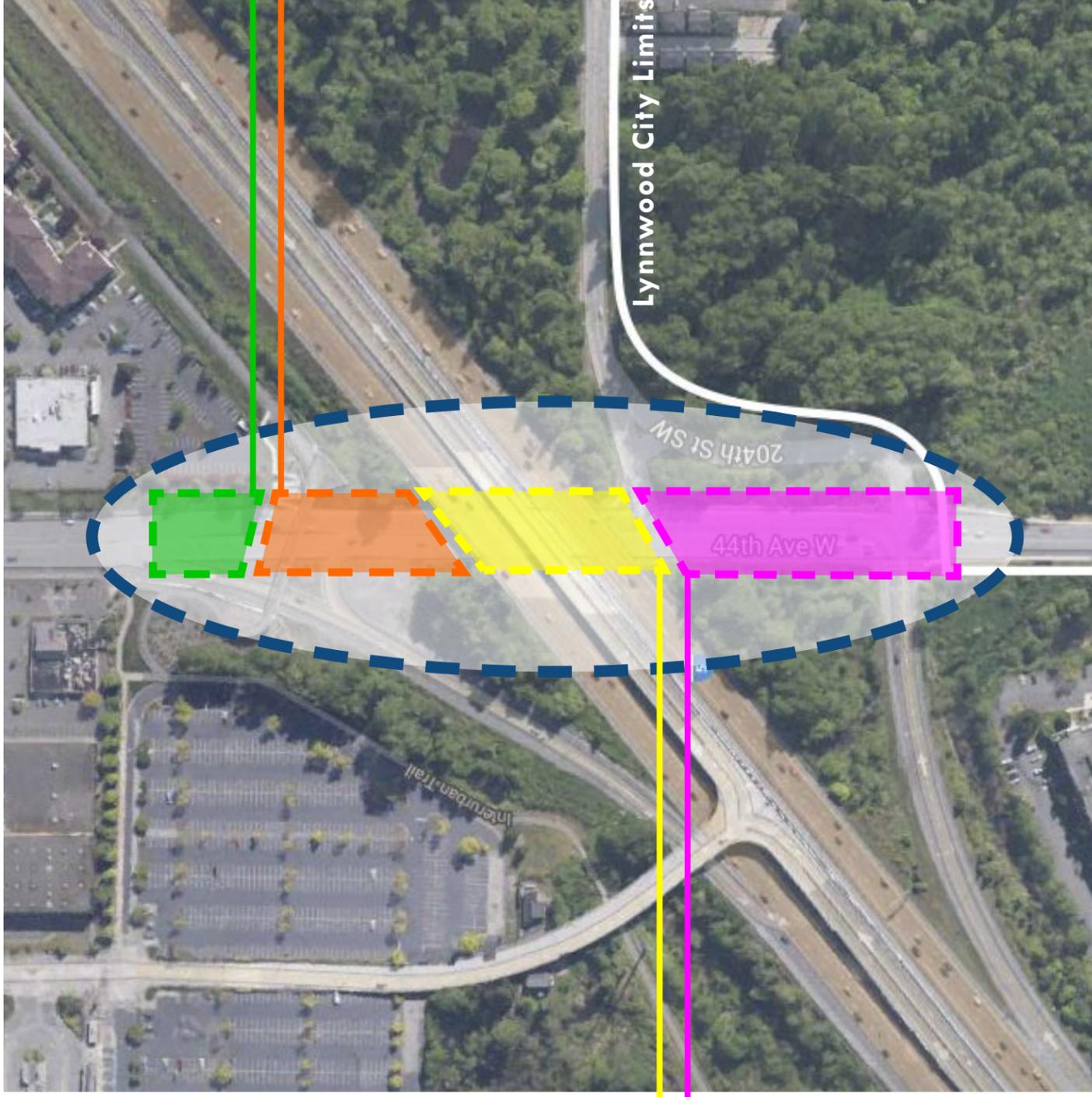
Gateway Portal



Gateway Threshold



Existing Gateway

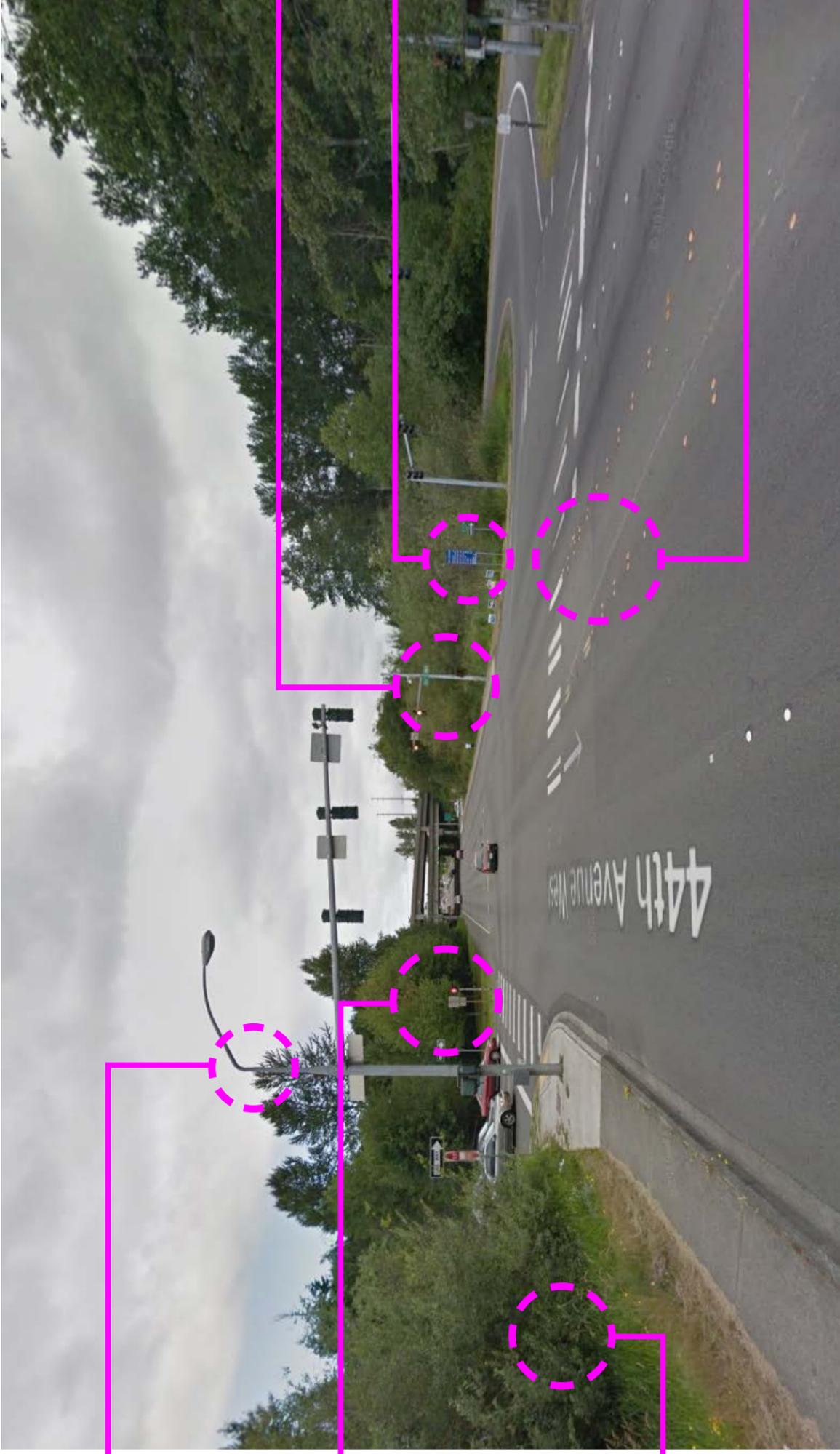


Gateway Approach | Existing

standard, annodized signal and street light poles not aesthetically pleasing, could be updated to new City Center poles

lawn area / open space allows for art or gateway enhancements

swale / wetlands, problematic to modify



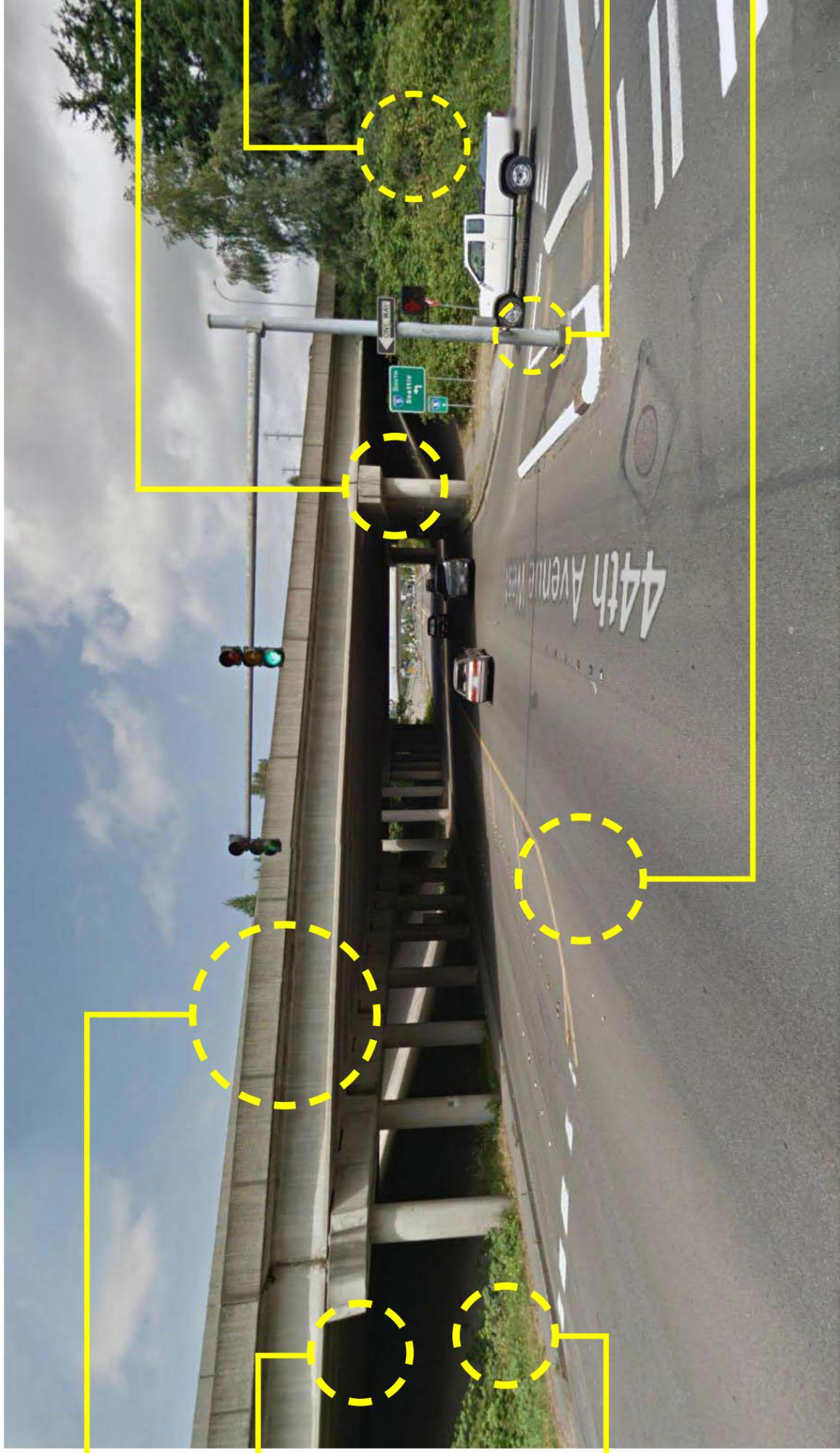
swale / wetlands, problematic to modify

Lynnwood wayfinding sign small, relatively diminutive at intersection

median space could become planted median

“Overall area visually separated from City Center; pedestrian connectivity confusing, experience needs to be improved.”

Gateway Portal | Existing



WSDOT interstate bridges, structures group must approve any enhancements

bridge creates dark and uninviting experience with heavy mass and dark shadows

landscape embankments part of bridge support and alternatives must be approved by WSDOT structures group

gray columns look worn

landscape embankments part of bridge support and alternatives must be approved by WSDOT structures group

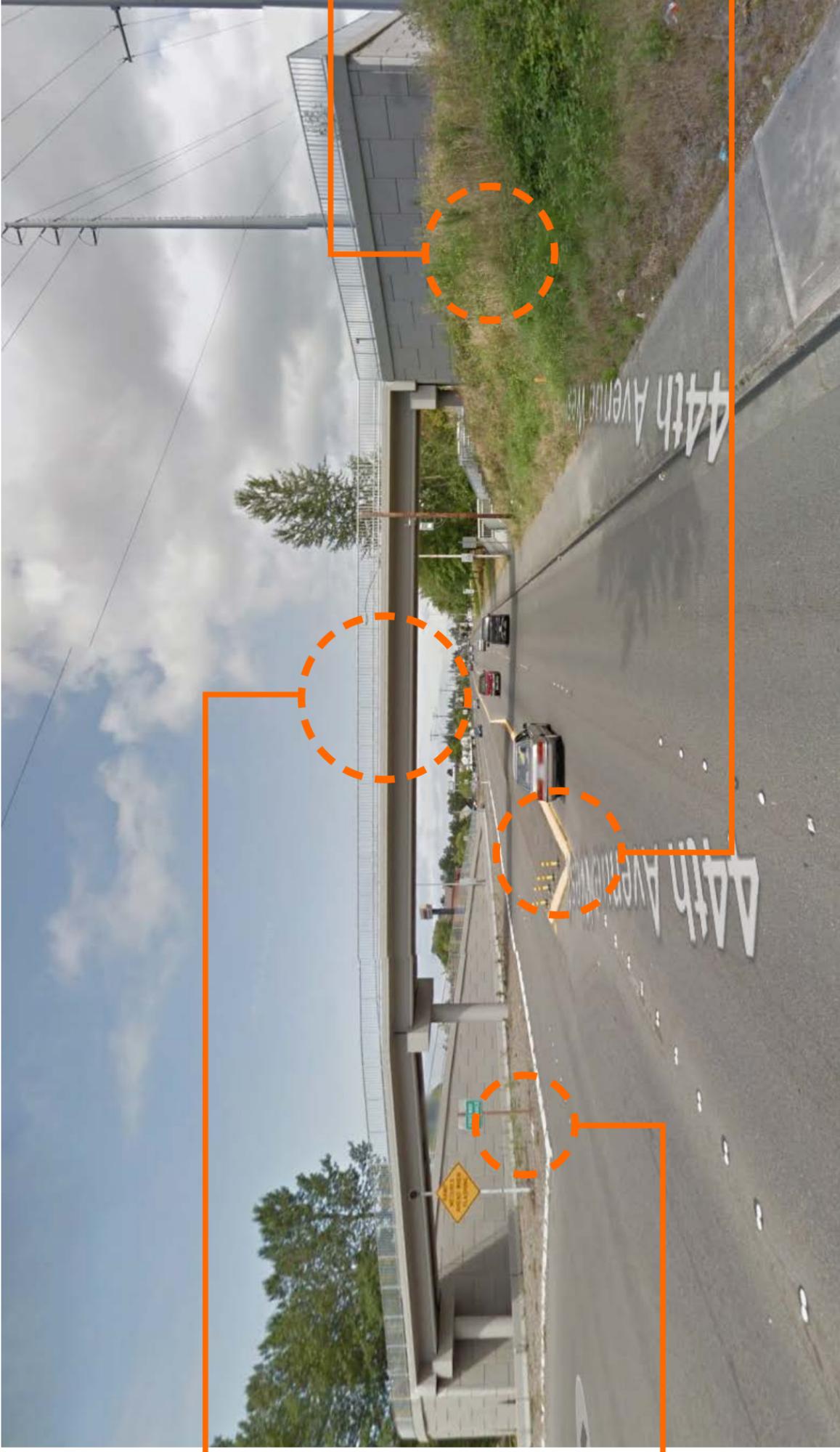
standard, annodized signal and street light poles blend with bridges

striped median could become planted median

“Overall area visually separated from City Center; pedestrian connectivity confusing, experience needs to be improved.”

Gateway Threshold | Existing

pedestrian bridge over street creates opportunity for gateway enhancements



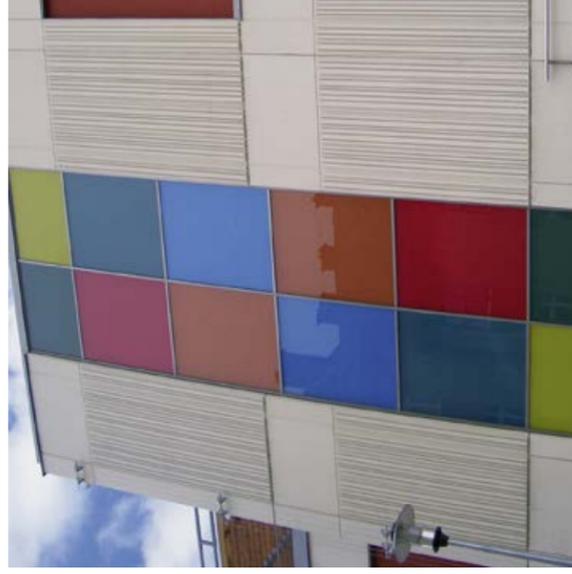
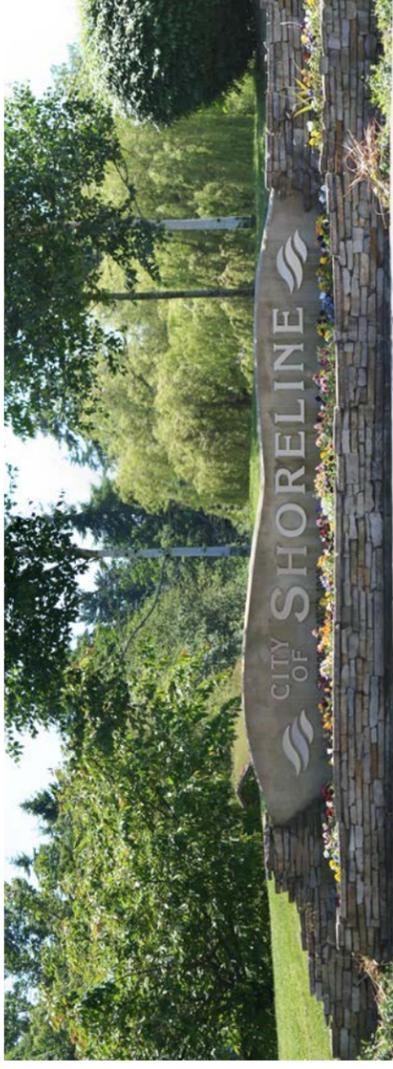
landscape slope, provides opportunity for embedded signage or art

paved, non-drivable space unattractive and not maintained, could be cleaned up and provides an opportunity for landscaping

curbed, paved median could be planted median

“City Center visible ahead.”

Example Gateway Elements



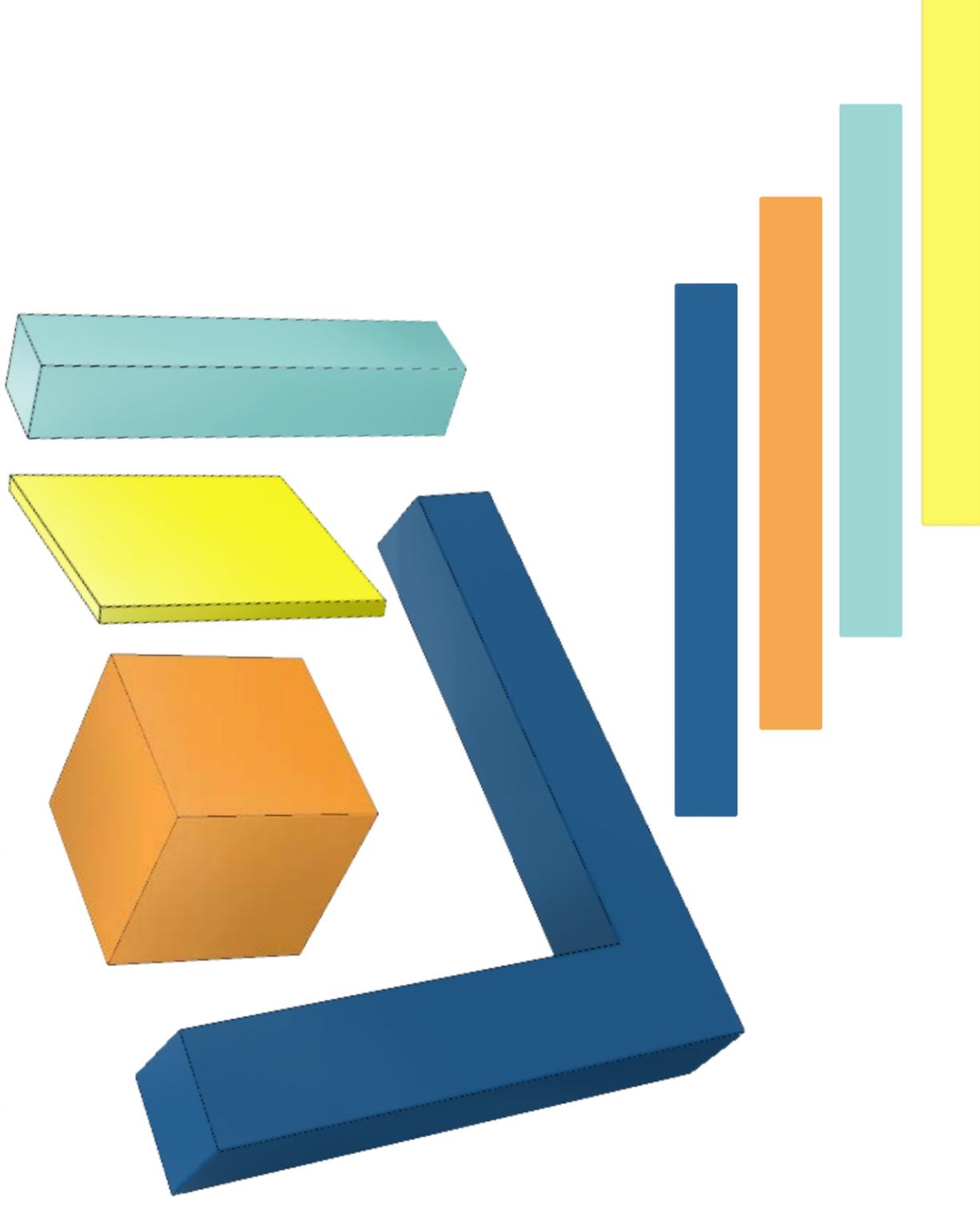
City Center Branding



The city logo provides a strong brand and image to the City Center, it evokes:

- contemporary
- crisp, clean lines
- commercial / business

The elements can become “building blocks” and urban design forms to guide, announce, celebrate and signify the City Center.



Gateway Approach | Possibilities

- new signal poles, “signal” City Center brand
- new landscaping adds color to the beginning of the gateway experience



- larger wayfinding sign more legible and helps guide visitors to Lynnwood destinations
- landscape median

Gateway Portal | Possibilities

- painting likely acceptable with WSDOT structures group and enlivens gateway experience
- lighting added to bridge structure in between the columns to illuminate pedestrian walkway; vibrant lighting added to underneath structure at top of embankment, to brighten, add color and visual interest
- colorful plantings add life to embankments

Bridge structure and columns with paint



- painted columns unify and clean up underpass, can also incorporate lighting and colored polycarbonate panels
- colorful plantings add life to embankments
- new signal poles soften space by adding color
- striped median retrofitted with colorful vegetation, can also pursue decorative pavement

Bridge structure and columns without paint



Gateway Portal | Possibilities

- down lighting added to the concrete beams of the underpass provides a more inviting, comfortable and safe space for pedestrians
- vibrant lighting at the top of the embankment provides visual interest and variety
- new concrete sidewalks establish safer pedestrian accessway

West walkway beneath interstate bridge



East walkway beneath interstate bridge



Gateway Threshold | Possibilities

- bridge treatments create visual and physical cues through painting and/or cladding with polycarbonate panels
- paved space softened with low maintenance landscaping, can also include art



- embedded city artwork, or signage, with colorful vegetation on slope
- median includes new landscaping to soften roadway section, can also include City Center signage



TRANSIT SHELTER

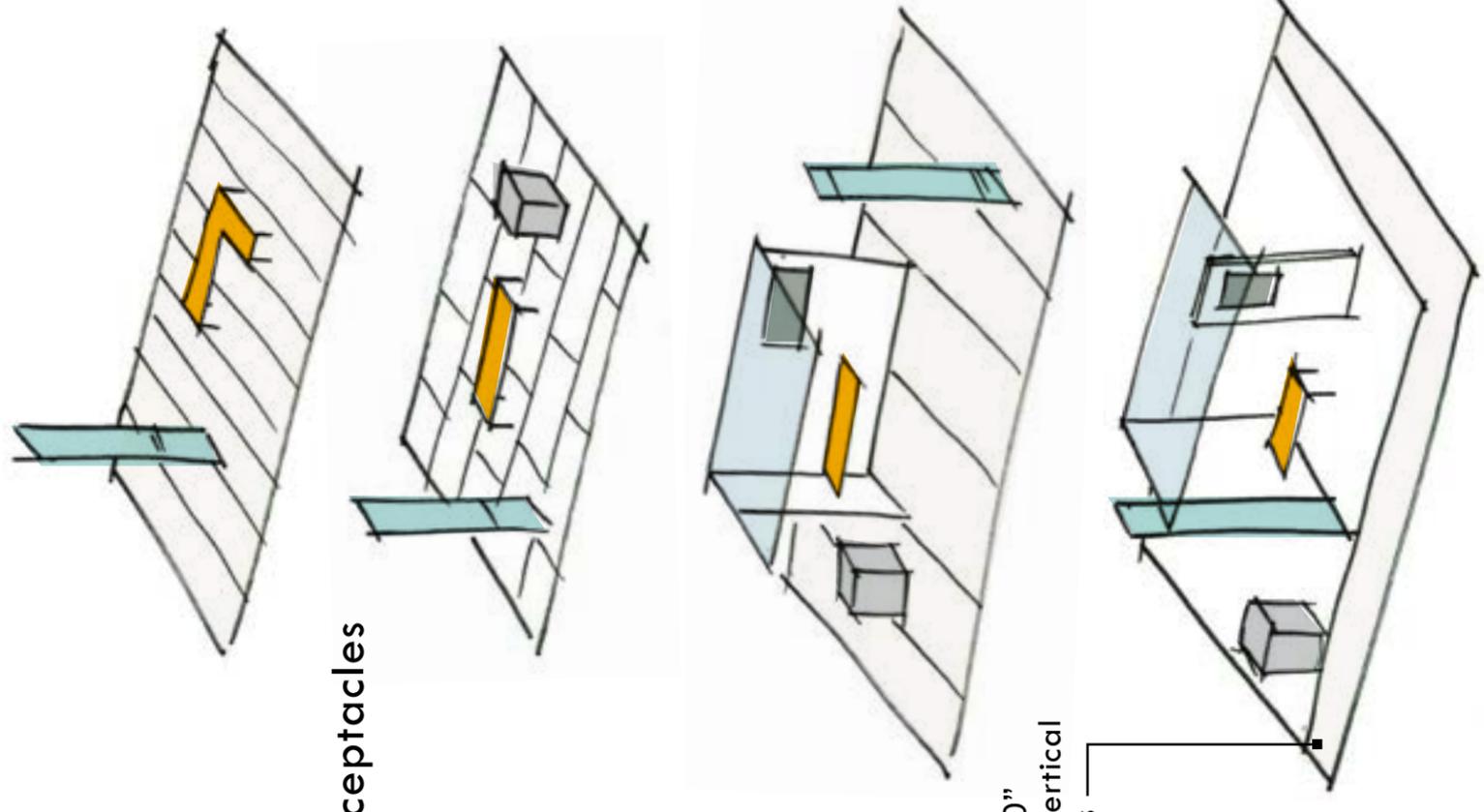
Existing Transit Shelters



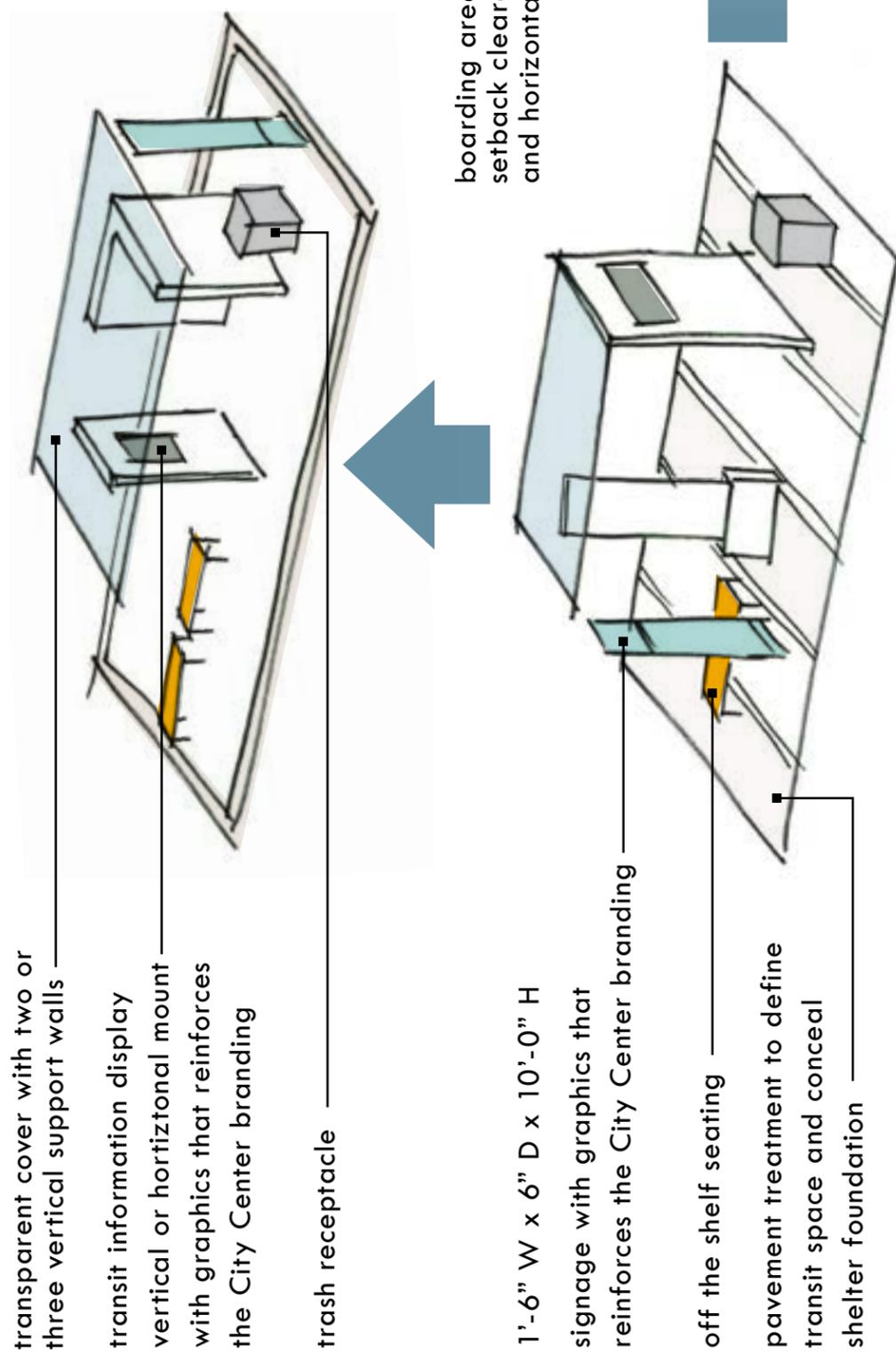
The transit elements make up a “kit of parts” which has the ability to:

- evoke the identity and contemporary character of the City Center
- be adapted and integrated at various sidewalk conditions
- be flexible for a wide range of users to maneuver around
- allow for the installation of signage, information displays, seating and trash receptacles

BASIC



FULL BUILD OUT OF PARTS



transparent cover with two or three vertical support walls
 transit information display vertical or horizontal mount with graphics that reinforces the City Center branding
 trash receptacle

boarding area with 4'-0" setback clearance for vertical and horizontal elements

1'-6" W x 6" D x 10'-0" H signage with graphics that reinforces the City Center branding
 off the shelf seating
 pavement treatment to define transit space and conceal shelter foundation

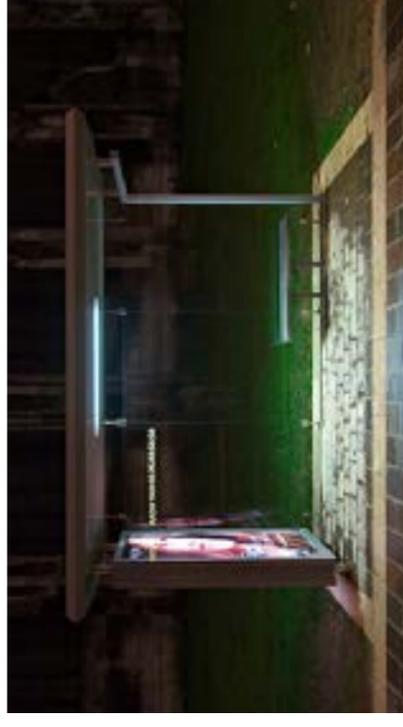
Design Concept A Transit Elements

- **Color Scheme:** all shades of gray, black and warm wood tones
- **Shelter Structure:** metal structure with shatter resistant glass, hardwood components, powder coated steel, stainless steel, brushed aluminum, frosted and/or clear outdoor grade acrylic sheets
- **Information:** LED display board with stainless steel enclosure
- **Signage:** LED internally lit pylon form, ledgable graphics with accent color and vinyl adhesive film
- **Seating:** integrated shelter seating, off the shelf backless benches, surface mounted
- **Trash Receptacle:** single or double units, anodized cast aluminum
- **Pavement:** shades of gray colored concrete and sawcut scoring



Design Concept B Transit Elements

- **Color Scheme:** all shades of gray and black
- **Shelter Structure:** metal structure with shatter resistant glass, powder coated steel, stainless steel, brushed aluminum, frosted and/or clear outdoor grade acrylic sheets
- **Information:** LED display screen with glass and aluminum enclosure
- **Signage:** LED internally lit pylon form, ledgible graphics with accent color and vinyl adhesive film
- **Seating:** integrated shelter seating, off the shelf backless benches, surface mounted
- **Trash Receptacle:** anodized cast aluminum with covered top
- **Pavement:** Rectangular concrete pavers in shades of gray



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Land Use Element - Draft

Agenda Item: E.2

Staff Report

- Public Hearing
- Work Session
- Other Business
- Information
- Miscellaneous

Staff Contact: Corbitt Loch, Deputy Director

The following report is repeated from the June 26, 2014 agenda. The four Commissioners at that meeting deferred substantive review of the draft Land Use Element to a future meeting so that additional members could participate. Refer to the two versions of the draft Land Use Element provided with the packet for the June 26, 2014 meeting.

Summary

The purpose of this agenda item is to introduce a “working draft” of the Land Use Element for the Commission’s initial review. The Commission is asked to provide feedback and guidance regarding the content, organization, and appearance of the draft Element. It is expected that several meetings will be needed for the Commission’s review of what will ultimately be a full update of the Land Use Element.

Policy Question(s)

Are the edits related to content, organization and appearance generally consistent with the Commission’s preferences?

Action

Provide direction to staff as appropriate.

Background

The Land Use Element is a fundamental chapter of the City’s Comprehensive Plan. In a sense, all of the other Comprehensive Plan Elements support and facilitate the goals and policies of the Land Use Element.

The draft Element provided now is an iterative draft with additional edits forthcoming regarding population and employment growth targets. Those edits are still being prepared. Meanwhile, the Commission is invited to review the many other changes prepared so far. The Commission’s feedback will help determine whether the proposed organization, format and writing style are

appropriate. This is important feedback as all of the Elements are in need of considerable updating, and staff intends to apply a uniform appearance and structure to each Element.

City staff is working to update the Comprehensive Plan in a manner that will facilitate review and participation by the Commission, other advisory groups, the general public, and the City Council. As required by GMA, this update is scheduled to be adopted by the City Council by June 30, 2015.

In February 2014, the Commission reviewed the first draft of the Environment Element. It is expected that the Environment Element will be reviewed a second time at the July meeting.

Staff's work on the 2015 Comprehensive Plan is guided by the following principals:

1. The primary objective is to ensure the Comprehensive Plan's consistency with current versions of the Growth Management Act (GMA), Multicounty Planning Policies (MPPs), and the Countywide Planning Policies for Snohomish County (CPPs). This includes accommodation of population and employment growth targets set forth by the CPPs.
2. Staff's identification of significant changes to Comprehensive Plan policies will help the general public and advisory groups understand and meaningfully participate in Lynnwood's consideration of this revised Comprehensive Plan. Without a highlighting of salient policy changes, readers would be obligated to read original and new versions in order to identify substantive edits.
3. With some re-organization, the Comprehensive Plan can be made more succinct without substantive policy change.
4. With some re-wording, the Comprehensive Plan text can be made more succinct without substantive policy change.

The existing Land Use Element is 35 pages in length, while the draft Element is 19 pages. Overall, there are relatively few, substantive changes proposed.

The existing Land Use Element is provided with "track changes" shown, and with annotations to explain the proposed change. Also provided is a clean version of the draft Land Use Element, with edits incorporated.

Previous Planning Commission / City Council Action

During past meetings, the Planning Commission has provided guidance upon:

1. General scope of the update and a "short-list" of substantive issues to be addressed.

2. Public participation plan for the 2015 Comprehensive Plan update.
3. Draft Environment Element.

Funding

None. The majority of work on the Comprehensive Plan will be completed using in-house resources.

Key Feature(s)

1. Creates a more-efficient organizational structure and more-concise wording.
2. “Track changes” version includes annotations.
3. Moves policies relating to aesthetics, urban design, historic preservation, etc. to a new Community Character Element.
4. Provides “placeholders” for policies/strategies relating to population and employment growth targets.

Adm. Recommendation

Review and provide direction on:

1. Proposed approach to organization, formatting and style.
2. A portion of the draft Land Use Element, as time allows (i.e., first ten pages)

Attachments

None. See the two draft versions of the Land Use Element provided with the June 26, 2014 packet.

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